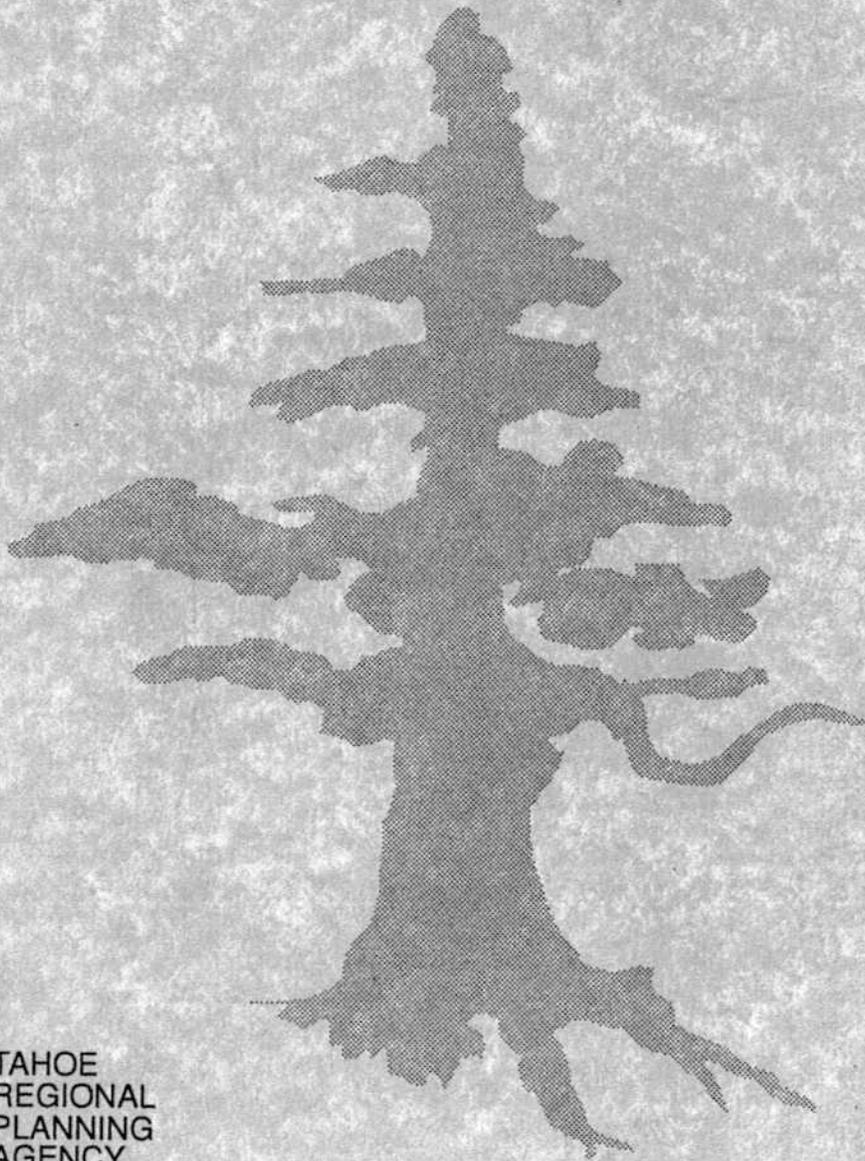


Regional Plan for the Lake Tahoe Basin

MEYERS COMMUNITY PLAN



October 27, 1993

REGIONAL PLAN FOR THE LAKE TAHOE BASIN



MEYERS COMMUNITY PLAN

**Adopted by the Tahoe Regional Planning Agency
Governing Board
May 27, 1987**

**Adopted by the El Dorado County
Board of Supervisors
November 5, 1987**

Meyers Community Plan Team

John Fellows, Chairman
Dena Schwarte, Vice-Chairman
Carl Fair
Ari Makinen
Sue Novasel
Chris Spitz
Shirley Taylor
Mark Witters
Elizabeth Eddins, El Dorado County
Andrew Strain, TRPA

Plan and Graphics

John Hitchcock, TRPA

Word Processing

Roxanne Duer, TRPA

Former Staff

Edward Crowley, El Dorado County
Ken Greenwood, El Dorado County
Don Sargent, TRPA

**Tahoe Regional Planning Agency
Reprinted June, 1998**

Meyers Community Plan

TABLE OF AMENDMENTS

1. Ordinance No. 95-14, Adopted December 20, 1995
2. Ordinance No. 98-14, Adopted May 27, 1998
3. Ordinance No. 98-19, Adopted August 26, 1998

Meyers Community Plan

TABLE OF CONTENTS

CHAPTERS

Chapter 1	INTRODUCTION	1-1
Chapter 2	LAND USE ELEMENT	2-1
Chapter 3	TRANSPORTATION AND CIRCULATION	3-1
Chapter 4	ENVIRONMENTAL CONSERVATION ELEMENT	4-1
Chapter 5	RECREATION ELEMENT	5-1
Chapter 6	PUBLIC SERVICE ELEMENT	6-1
Chapter 7	IMPLEMENTATION ELEMENT	7-1

APPENDICES

A.	Community Plan Design Review Guidelines	A-1
B.	Community Plan Substitute Sign Standards	B-1
C.	Community Plan Commercial Floor Area Allocation Procedures	C-1
D.	Assessor's Parcel Numbers Listed by Land Use District.....	D-1

Meyers Community Plan

LIST OF FIGURES

Figure 1-1	Community Plan Area	1-2
Figure 1-2	Meyers Vicinity Map	1-4
Figure 1-3	Plan Area Site Analysis.....	1-7
Figure 1-4	Land Use Concept Plan	1-9
Figure 2-1	Land Use Districts.....	2-3
Figure 3-1	Transportation and Circulation	3-7
Figure 4-1	Water Quality Improvements	4-3
Figure 4-2	Community Design Plan.....	4-7
Figure 4-3	Community Plan Land Capability	4-11
Figure 6-1	Public Services	6-5

Meyers Community Plan

LIST OF TABLES

Table 2-1	Performance Standards for Stationary or Industrial Noise Source	2-14
Table 4-1	Comparison of 1987 and Community Plan Land Capability Maps	4-10
Table 7-1	Community Plan Threshold Target Monitoring Summary	7-13

Chapter I

INTRODUCTION

A. BACKGROUND

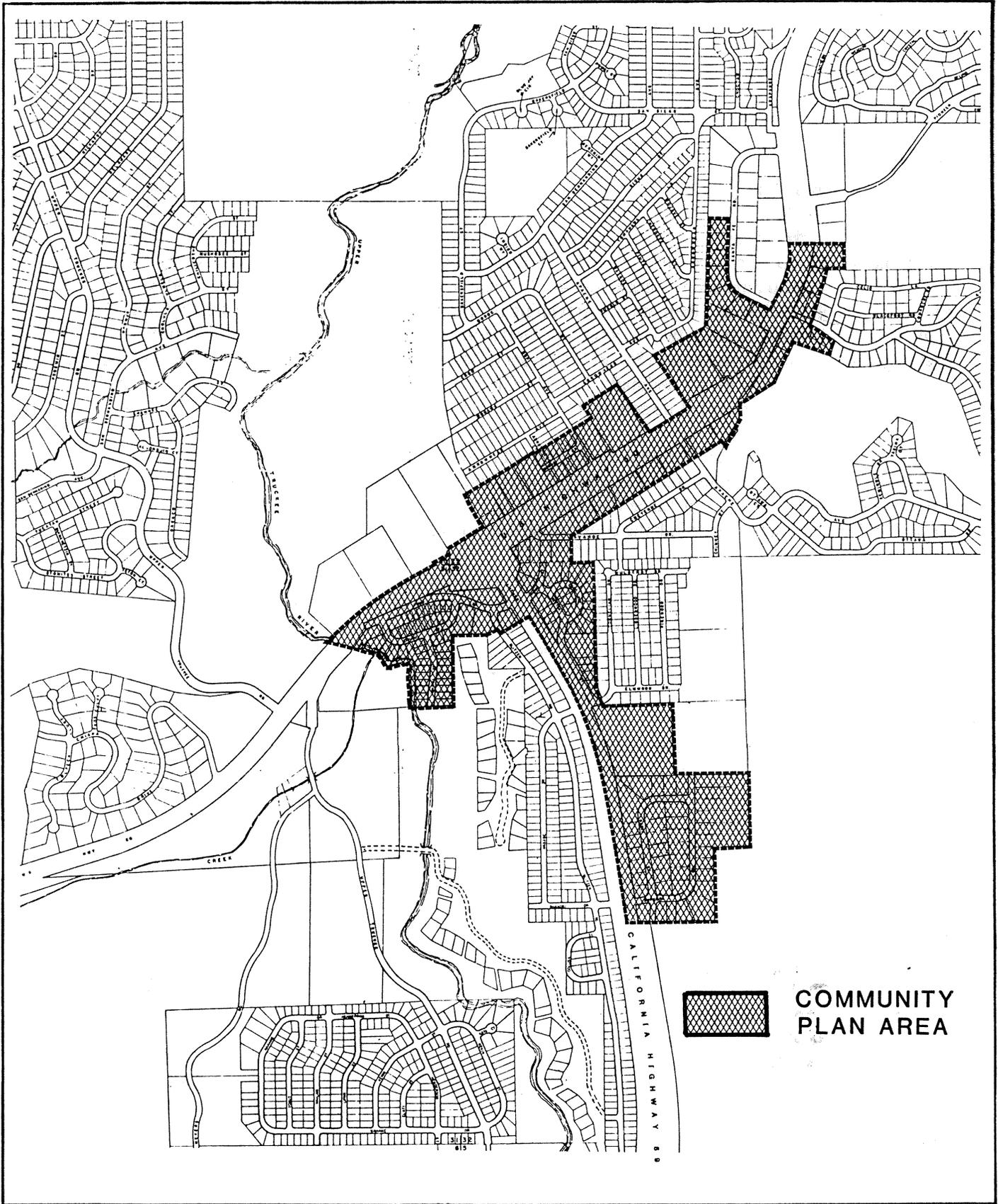
Meyers was first established in the 1850's as a way station near its present location in the lower Lake Valley along the Upper Truckee River. In 1859, Martin Smith, Meyers' original developer, sold the station to Yank Clement, who renamed it Yank's Station. The station provided food, lodging, water and pasture to the thousands of travelers and their animals travelling over Echo Summit along the Great Bonanza Road. Yank's Station included a hotel, two saloons, a general store, a blacksmith shop, a cooperage, private homes and stables and barns. From 1860 to 1861 it served as a remount station for the Pony Express and is listed as a California Historical Landmark. In 1873, Clement sold the establishment to George Meyers who owned it for thirty years before selling it to the Celio family.

During the 1960's, the area around Meyers was part of a grand residential subdivision plan originally developed by two corporations, Tahoe Paradise Homes and Tahoe Paradise Properties, Inc. The new neighborhoods were to be called Tahoe Paradise. Since that time the entire area is referred to as either Meyers or Tahoe Paradise, although the commercial district is generally identified as Meyers.

Today, Meyers still functions as a way station for visitors and the gateway to the Lake Tahoe area. The commercial area along Highways 50 and 89 still retains the name of Meyers while the surrounding residential neighborhoods are generally known as Tahoe Paradise. In addition, Meyers serves several thousand surrounding residents by providing convenience retail goods and services. Unlike other communities in the Tahoe Basin which are located along the lake's shore, Meyers is separated from intense commercialization. It retains its own character while providing a variety of land uses.

This document represents the future land use and development plan for the existing Meyers commercial district and includes all lands within TRPA Plan Area 125, Meyers Commercial (Figure 1-1). The plan area is roughly 155 acres in size, approximately 50 of which are located in the Meyers Industrial Tract located south of the U.S. 50 and California 89 intersection in the Upper Lake Valley. The remaining 105 acres are generally devoted to commercial retail, service and light industrial uses. Meyers contains several local, state and federal public service land uses and serves as a hub for public services in unincorporated El Dorado County.

Meyers is surrounded by spectacular mountain peaks, mature pine and fir forests. The Upper Truckee River forms the southern edge of the plan area. Other edges of Meyers are bounded by the Tahoe Paradise Golf Course, residential neighborhoods, Washoe Meadows State Park and national forest lands (Figure 1-2). Perhaps the most dominant element within the Meyers landscape is the U.S. Highway 50 right-of-way. Originally planned to contain a high-speed freeway, the right of way is up to 280 feet wide in places and contains only two and three lanes of actual roadway. The result is a large area of disturbed and undeveloped public "no man's land" which adversely impacts the sense of community. The freeway concept has been discarded as inappropriate for the Tahoe Region; however, the right-of-way remains. Future use of the right of way provides the community plan with opportunities as well as constraints.



Meyers Community Plan
COMMUNITY PLAN AREA

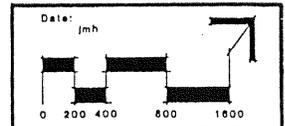
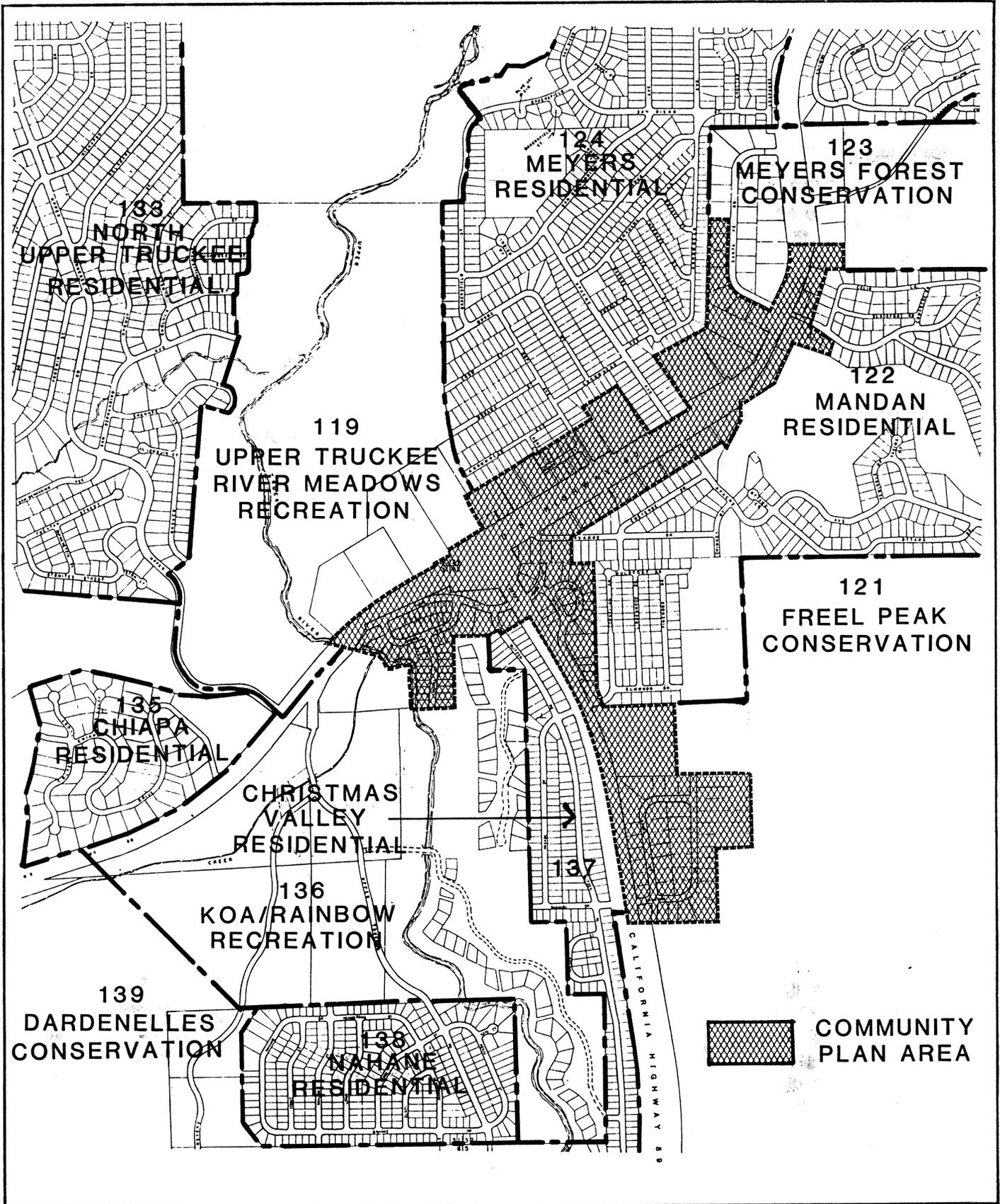


Figure 1-1. Community Plan Area



Meyers Community Plan VICINITY MAP

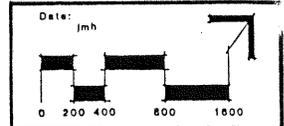


Figure 1-2. Meyers Vicinity Map

B. PURPOSE OF THE COMMUNITY PLAN

The community plan for Meyers is intended to serve as the comprehensive land use and development plan through the year 1997. The plan establishes goals and objectives, special policies, programs, and strategies for funding and implementation. It provides an important opportunity to improve the form and function of the commercial neighborhood and help establish a sense of place.

The Community Plan contains a new Community Plan Area Statement which replaces the existing TRPA Plan Area Statement 125, Meyers Commercial. It amends the El Dorado County General Plan and designates land uses within the boundaries of the Community Plan area. The Community Plan will be incorporated into the County's 2010 General Plan upon adoption. Following 1997, the plan will be updated regularly to provide a blueprint for continued prosperity and improvement of the community.

C. COMMUNITY PLAN SITE ANALYSIS

The planning process began with a general site analysis of the plan area (Figure 1-3). The analysis identifies existing conditions, both positive and negative, within the plan area. It also identifies opportunities and constraints to the development of future land uses and the redevelopment of existing uses. A summary is listed below.

Site Analysis Opportunities

- ❑ Location: Gateway to the Tahoe Basin, first stop for visitors entering over Echo Summit and Luther Pass; historic Pony Express stop/way station.
- ❑ Setting: Surrounded by spectacular mountain vistas; many opportunities to access nearby outdoor recreation; Upper Truckee River is an under used resource.
- ❑ Sense of Community: Many business operators and nearby residents identify with the community of Meyers.
- ❑ Surrounding Neighborhoods: Convenience retail and service needs create a market niche to serve residential neighborhoods.
- ❑ Vacant Land: An adequate supply of privately owned, vacant, high capability land exists within the plan area for future development.

Site Analysis Constraints

- ❑ Existing Land Use Pattern: Commercial strip, much of it in a filled SEZ.
- ❑ U.S. 50 Right-Of-Way: Large, apparently abandoned area of land; existing businesses set too far back; too many public directional and private business signs in the ROW; through traffic travels too fast.
- ❑ Appearance of Neglect: Many business in need of renovation; County and TRPA do not enforce existing land use and site development regulations.
- ❑ Winter Snows: Plowed snow in front of businesses blocks visibility; queues of vehicles waiting to go over Echo Summit snarl local traffic.

D. COMMUNITY PLAN GOALS

In addition to the goals of both the El Dorado County General Plan and TRPA Regional Plan, the community plan intends to achieve the following goals. Specific objectives, policies and actions necessary to implement the goals are contained in Chapters 2 through 7 of this document.

θ Land Use and Economic Diversity

Maintain the long term economic health and stability of the plan area by providing a diverse mix of commercial, recreational and public service land uses in five separate land use districts which serve both residents and tourists (Chapter 2).

θ Community Design

Implement a comprehensive community design program which will improve the visual quality of the commercial area and help Meyers establish a sense of permanence (Chapter 2).

θ Transportation and Circulation

Redevelop the transportation system within the community plan area to reduce reliance on the private automobile, improves circulation and provide opportunities to experience Meyers as a pedestrian or cyclist (Chapter 3).

θ Environmental Conservation

Improve environmental values in Meyers as part of implementing the community plan (Chapter 4).

θ Recreation

Expand recreational opportunities within the community plan area and provide access to nearby recreation (Chapter 5). Public Services: Support development of public service projects within the plan area which support the community (Chapter 6). Implementation: Develop realistic implementation methods to ensure the community plan's long term success (Chapter 7).

E. THE LAND USE CONCEPT PLAN

The plan's starting point is the conceptual land use plan shown in Figure 1-4.

Plan Concept

The plan's concept is to enhance the identity and image of Meyers as both a community center and a gateway to Lake Tahoe. This will be achieved through changes to the existing strip commercial land use pattern together with aggressive implementation of a Community Design Program for all uses. The Plan advocates several actions to visually and physically reduce the dominating U.S. Highway 50 corridor. The Plan recommends environmental improvement projects to improve and enhance water quality, stream environment zone restoration, visual and scenic quality, transit service and pedestrian/bicycle facilities. A new Meyers Town Council will be created to guide the Plan's implementation.

The plan's land use element will discourage perpetuating the strip commercial land use pattern along U.S. 50 by consolidating most additional commercial development into three land use districts. A total of five land use districts are created to replace the existing Plan Area Statement and County zoning. Each district is described in Chapter 2 and contains a specific development theme and list of permissible land uses.

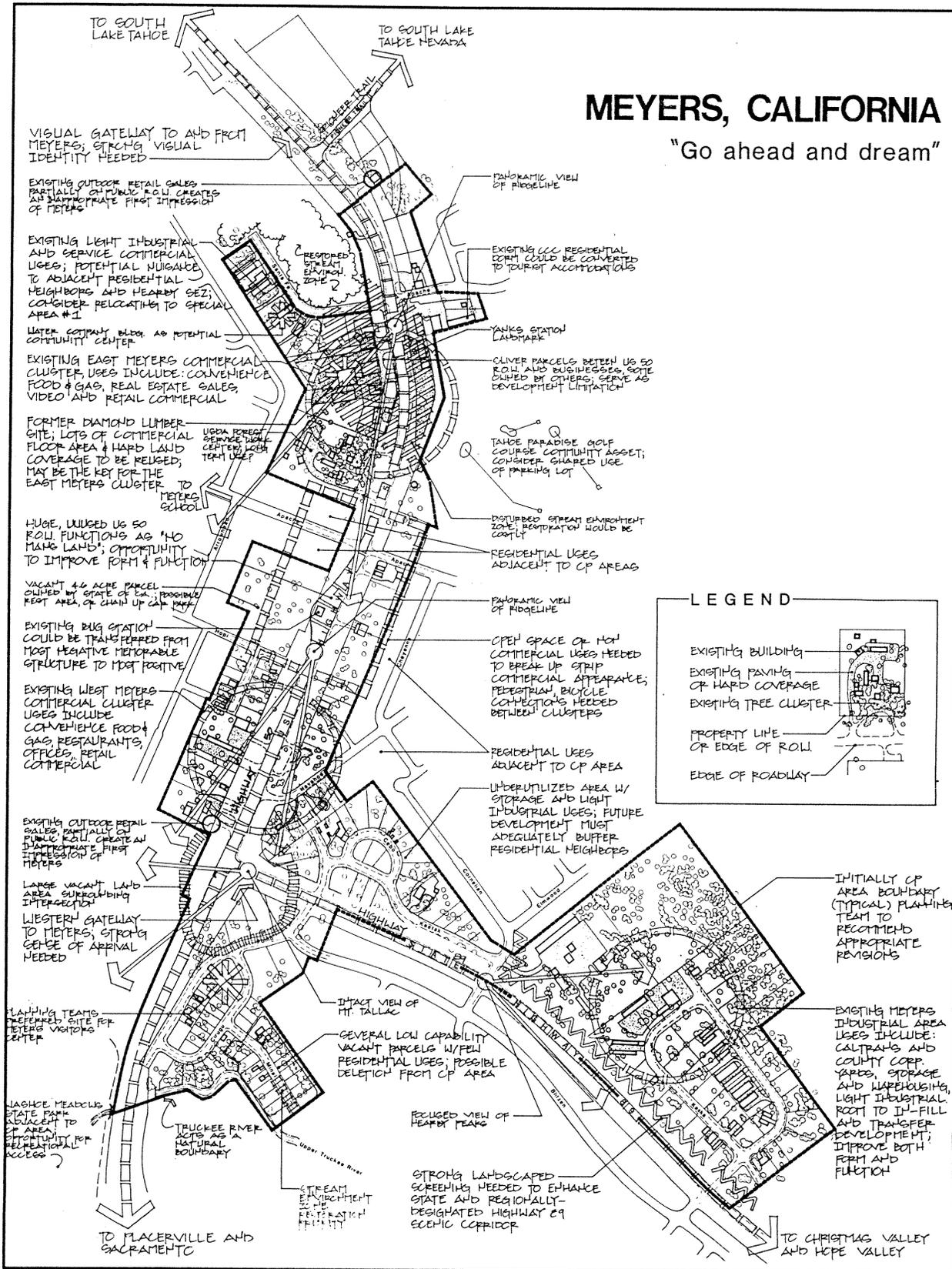
The Plan advocates implementing several environmental improvement projects in the areas of water quality, stream environment zone restoration, visual and scenic quality, transportation and circulation. They are highlighted on the Land Use Concept Plan (Figure 1-4) and described in subsequent chapters of the plan.

The Land Use Concept Plan illustrates generalized land use patterns and identifies needed community-wide improvements. Improvements come in two forms: public improvements paid for collectively for all to enjoy; and private improvements funded by private resources intended to produce a more attractive (and profitable) business setting. Specific plan policies and standards are implemented (enforced) as part of each project, public or private.

Please note that the Land Use Concept Plan is for illustration purposes only. It does not require nor approve any specific projects.

MEYERS, CALIFORNIA

"Go ahead and dream"



Meyers Community Plan PLAN AREA SITE ANALYSIS

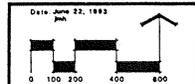
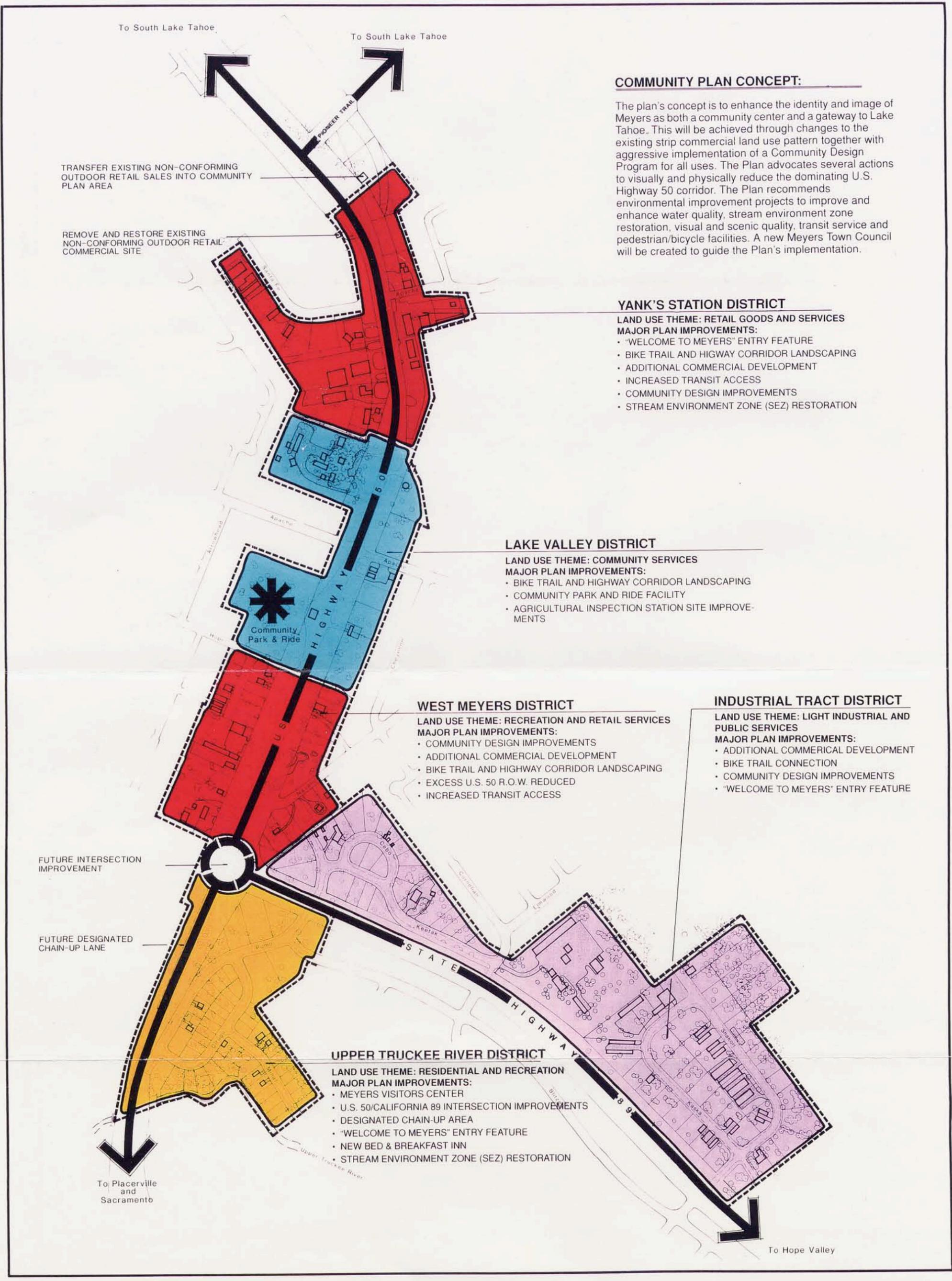


Figure 1-3. Plan Area Site Analysis



COMMUNITY PLAN CONCEPT:

The plan's concept is to enhance the identity and image of Meyers as both a community center and a gateway to Lake Tahoe. This will be achieved through changes to the existing strip commercial land use pattern together with aggressive implementation of a Community Design Program for all uses. The Plan advocates several actions to visually and physically reduce the dominating U.S. Highway 50 corridor. The Plan recommends environmental improvement projects to improve and enhance water quality, stream environment zone restoration, visual and scenic quality, transit service and pedestrian/bicycle facilities. A new Meyers Town Council will be created to guide the Plan's implementation.

YANK'S STATION DISTRICT

- LAND USE THEME: RETAIL GOODS AND SERVICES**
MAJOR PLAN IMPROVEMENTS:
- "WELCOME TO MEYERS" ENTRY FEATURE
 - BIKE TRAIL AND HIGHWAY CORRIDOR LANDSCAPING
 - ADDITIONAL COMMERCIAL DEVELOPMENT
 - INCREASED TRANSIT ACCESS
 - COMMUNITY DESIGN IMPROVEMENTS
 - STREAM ENVIRONMENT ZONE (SEZ) RESTORATION

LAKE VALLEY DISTRICT

- LAND USE THEME: COMMUNITY SERVICES**
MAJOR PLAN IMPROVEMENTS:
- BIKE TRAIL AND HIGHWAY CORRIDOR LANDSCAPING
 - COMMUNITY PARK AND RIDE FACILITY
 - AGRICULTURAL INSPECTION STATION SITE IMPROVEMENTS

WEST MEYERS DISTRICT

- LAND USE THEME: RECREATION AND RETAIL SERVICES**
MAJOR PLAN IMPROVEMENTS:
- COMMUNITY DESIGN IMPROVEMENTS
 - ADDITIONAL COMMERCIAL DEVELOPMENT
 - BIKE TRAIL AND HIGHWAY CORRIDOR LANDSCAPING
 - EXCESS U.S. 50 R.O.W. REDUCED
 - INCREASED TRANSIT ACCESS

INDUSTRIAL TRACT DISTRICT

- LAND USE THEME: LIGHT INDUSTRIAL AND PUBLIC SERVICES**
MAJOR PLAN IMPROVEMENTS:
- ADDITIONAL COMMERCIAL DEVELOPMENT
 - BIKE TRAIL CONNECTION
 - COMMUNITY DESIGN IMPROVEMENTS
 - "WELCOME TO MEYERS" ENTRY FEATURE

UPPER TRUCKEE RIVER DISTRICT

- LAND USE THEME: RESIDENTIAL AND RECREATION**
MAJOR PLAN IMPROVEMENTS:
- MEYERS VISITORS CENTER
 - U.S. 50/CALIFORNIA 89 INTERSECTION IMPROVEMENTS
 - DESIGNATED CHAIN-UP AREA
 - "WELCOME TO MEYERS" ENTRY FEATURE
 - NEW BED & BREAKFAST INN
 - STREAM ENVIRONMENT ZONE (SEZ) RESTORATION

**Meyers Community Plan
 LAND USE CONCEPT PLAN**



Figure 1-4. Land Use Concept Plan

F. SUMMARY OF COMMUNITY PLAN PROGRAMS AND ACTIONS

Transportation and Circulation

- Ø Highway 50 Improvements. Improvements include constructing the Pat Lowe Memorial Bike Trail with future connections to surrounding recreation opportunities, addition of a center landscaped median with turn pockets along U.S. 50 through most of Meyers, better organization and some elimination of chain up areas along U.S. 50 and reduction/consolidation of existing directional signage which is cluttered and poorly located. Sale of portions of the existing non-operational right-of-way to adjacent property owners is an important land use improvement to reduce the corridor's dominance.
- Ø Transit and Parking Improvements. Improvements include using shared driveways and parking to encourage people to get out of their cars and reduce highway friction, increased transit service and stops by STAGE and other service providers, a new community park & ride facility and transit shelters at all transit stops.

Community Design

- Ø Community Design Program. Improvements recommended by the program include implementing a comprehensive community design program to improve the appearance of new and existing structures and land uses. It addresses architecture, site planning, landscaping, signage, lighting, and outdoor advertising. It advocates creating a public gateway into the community using public design features.

Environmental Improvements

- Ø Water Quality Improvements. Installation and maintenance of Best Management Practices on public and private lands. Removal and restoration of disturbed lands.
- Ø Stream Environment Zone (SEZ) Restoration. Restore existing disturbed SEZ lands in conjunction with the Pat Lowe Bike Trail along U.S. 50, restore SEZ lands behind Yank's Station and at the end of Minal Street cul-de-sac.
- Ø Protection of Western Juniper Trees. Conservation of all existing Western juniper trees as part of project development.
- Ø Scenic Quality Improvements. Implement site planning improvements identified in TRPA's 1989 Scenic Quality Improvement Program (SQIP) and the Community Design Program to attain a travel route threshold rating of at least 16 for portions of travel route unit #36 within the community plan area. Preserve existing views from Meyers to the surrounding background landscape.

Recreation Facilities

- Ø Meyers Visitors Center. The interagency visitors center will serve as a primary source of information for visitors entering the Tahoe basin. In 1992, an interim visitors center opened at the Little Switzerland site. The permanent facility will include access and interpretive opportunities to the adjacent Upper Truckee River.

- θ Tahoe Paradise Golf Course. Although not within the community plan, the existing Tahoe Paradise golf is close enough to the Yank's Station land use district to be an important attraction in terms of bringing people to the area. Redeveloping Yank's Station or the golf course should recognize and improve the connection.

Public Service Facilities

- θ Meyers is home to several local and regional public service uses. Many of the existing uses have expressed a desire to expand or upgrade their facilities. In particular, development of a new, full service post office is proposed to be located in either the Yanks Station or the West Meyers district. Architectural and site planning improvements to the existing state agricultural inspection station are recommended.

Chapter II

LAND USE ELEMENT

The Land Use Element is a supplement to the Land Use Element of the TRPA Goals and Policies Plan. This Element, consistent with the Regional Plan, establishes land use regulations for the community plan area. It revises and replaces the existing plan area statement 124, Myers Commercial. It establishes land use policy for El Dorado County by amending the County's 1969 General Plan. The revised Community Plan Area Statement is located in Section C of this chapter.

A. LAND USE REGULATIONS

Community planning is an option in which the local community in partnership with TRPA and local government may prepare their own plans and, to some extent, their own standards.

Listed below is a brief summary of policies and ordinances applicable to the Meyers Community Plan. In general, ordinances and standards of the El Dorado County Code and TRPA Code of Ordinances apply except for the following:

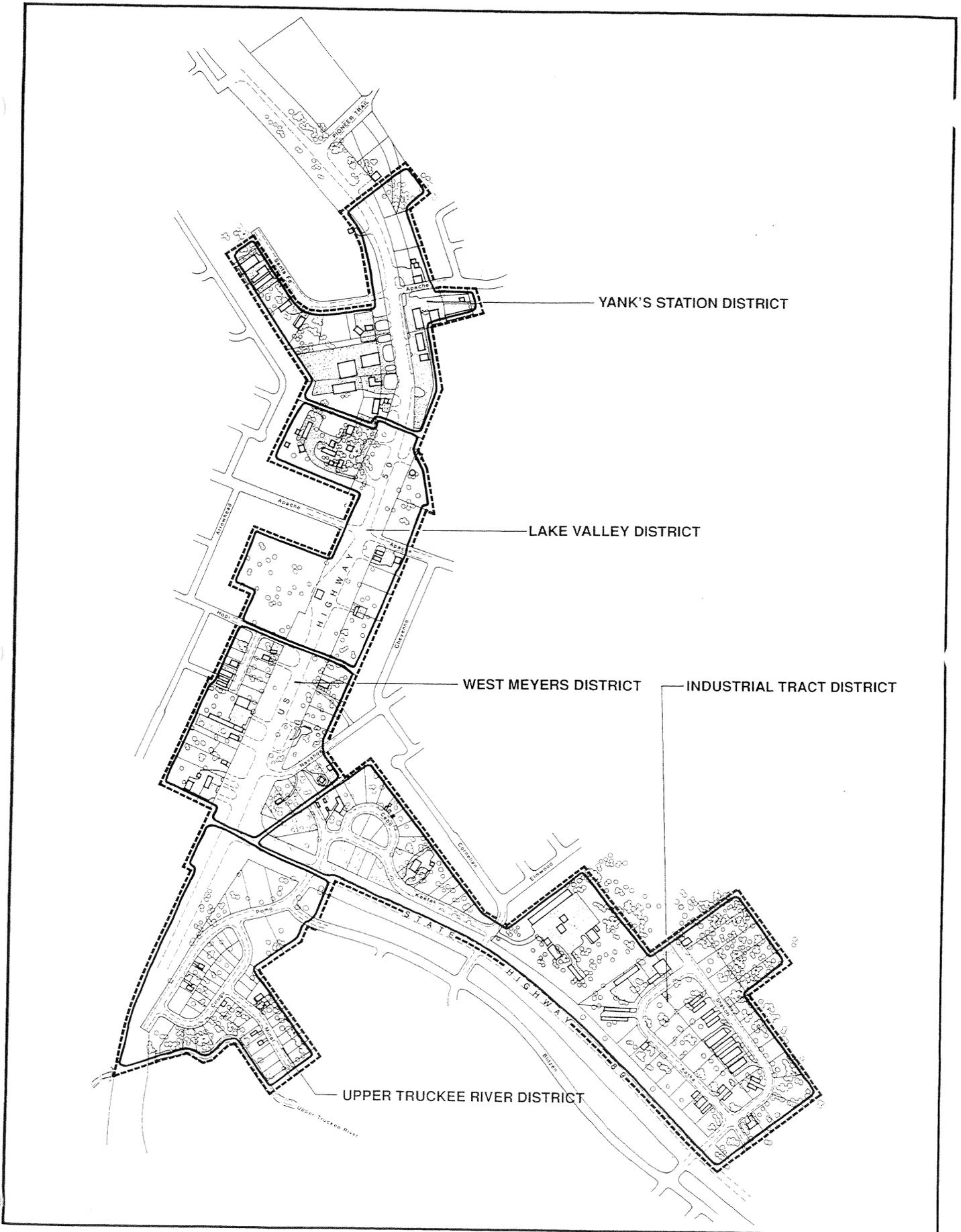
1. Land Use and Development Policies of the six community plan elements (Chapters 2 through 7);
2. Community Plan Area Statement (Chapter 2);
3. Community Plan Design Review Guidelines (Appendix A);
4. Community Plan Substitute Sign Standards (Appendix B); and
5. Commercial Floor Area Allocation Procedures (Appendix C).

B. COMMUNITY PLAN LAND USE DISTRICTS

All lands within the plan area are assigned to one of five separate land use districts. See figure 2-1. Within the overall plan, each district is intended to serve a specific purpose, has a defined land use theme, permits specific land uses and is targeted to receive certain public and private improvements. Each district is described below.

1. Yank's Station Land Use District (Special Area #1). Yank's Station is one of two primary commercial cores of the plan area. The existing Yank's Station complex is located on the site of the historic way station of the same name described in Chapter 1. The district's land use theme is retail goods and services. It is nearly built out. Very little high capability, vacant land exists. It is expected to continue to provide retail goods and services to residents and visitors generally through expansion or intensification of existing uses. The area around Santa Fe Street contains automotive repair and light industrial uses. Santa Fe Street uses should improve the visual and physical buffer with the adjacent neighbors in Plan Area 124.
2. Lake Valley Land Use Districts (Special Area #2). The Lake Valley district contains nine parcels, only two of which have commercial uses. The district's theme is community services. Public services and the community park and ride are expected to be predominant land uses.

3. West Meyers Land Use District (Special Area #3). West Meyers is the second primary commercial core in the plan area. Its land use pattern is strip commercial, and provides convenience goods and service to visitors and nearby residents. The district's land use theme is recreation and retail services. It contains the existing post office and interim visitors center. Several high capability, vacant parcels exist on which to develop new uses. Due to its proximity to the U.S. 50/California 89 intersection it serves as a last stop for convenience goods and services, especially recreation rentals, for people travelling to recreation areas.
4. Industrial Tract Land Use District (Special Area #4). The district is located along California 89, south of the U.S. 50 intersection. It contains light industrial uses, storage and warehousing and several public service yards. Its subdivision pattern resembles the form of an industrial park, however, it is developed as individual sites. The district's land use theme is light industrial and public services. Several vacant, high capability parcels exists. It is the targeted receiving area for new and relocated uses consistent with its theme, particularly those which may be transferred from the airport area along U.S. 50.
6. Upper Truckee River Land Use District (Special Area #5). The district is located south and west of the U.S. 50/California 89 intersection along the Upper Truckee River. It is the gateway to Meyers and the Tahoe Basin for eastbound travellers arriving over Echo Summit. Several single family homes are the only existing development within the district. Its land use theme is residential and recreation uses. The plan identifies the area as the preferred site for a permanent Meyers Visitors Center. Residential uses along Minal Street are expected to remain.



Meyers Community Plan LAND USE DISTRICTS

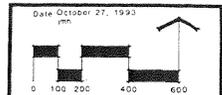


Figure 2-1. Land Use Districts

C. MEYERS COMMUNITY PLAN AREA STATEMENT

The Community Plan Area Statement supersedes TRPA's existing Plan Area Statement 125, Meyers Commercial and amends El Dorado County's 1969 General Plan.

PLAN DESIGNATION:

Land Use Classification	COMMERCIAL/PUBLIC SERVICE
Management Strategy	MITIGATION
Special Designations	TDR RECEIVING AREA FOR: 1. Existing Development 2. Multi-Residential Units (Special Area #3 Only) SCENIC RESTORATION AREA

DESCRIPTION:

Location: The community plan area includes the commercial and public service uses along U.S. Highway 50 and the Industrial Tract area along California 89. It is located on TRPA Maps F-21 and F-22.

Existing Uses: Meyers was developed during the 1960s and 1970s and today typifies a traditional strip commercial land use pattern. It presently contains convenience retail/gas markets, Yank's Station, small restaurants, miscellaneous office and retail uses. The Industrial Tract contains storage and warehouses and county and state highway maintenance facilities. Several local, state and federal public services facilities are located throughout the plan area.

Existing Environment: The plan area is approximately 155 acres in size. The majority of it lies in a relatively flat, alpine meadow known as the Lake Valley, bordered on one side by the Upper Truckee River. The Industrial Tract is located in the adjacent Upper Lake Valley, drained by the Upper Truckee River. The area's geomorphology consists of alluvial lands and outwash deposits. Land capability mapping is shown in Chapter 4.

PLANNING STATEMENT: Meyers serves as the gateway to the southern end of the Lake Tahoe Basin. It should continue to serve as a commercial and public service center for El Dorado County. It should serve as an access point to surrounding recreational opportunities on public and private lands.

PLANNING CONSIDERATIONS:

1. As the gateway to the Lake Tahoe basin, Meyers provides a clear sense of entry and arrival to one of the nation's premier resort areas.
2. The extremely wide U.S. 50 right-of-way has acted as a constraint to the establishment of appropriate and compatible commercial land uses and to the attainment and maintenance of environmental values. There are numerous illegal encroachment into the right-of way by adjacent businesses. This has perpetuated the strip commercial appearance.

3. A community design program will foster architectural, site planning and signage improvements. The improvements will assist in improving the travel route rating scenic threshold for Roadway Unit 36.
4. Improved transit service between Meyers and other destinations around the south shore is needed.
5. A land use pattern which concentrates commercial uses is needed to eliminate the existing strip commercial appearance and function.
6. U.S. 50 and California 89 are designated State Scenic Highways and TRPA Regional Scenic Highway Corridors.
7. Recognize the adjacent Tahoe Paradise Golf Course and clubhouse as an integral part of the Yank's Station commercial core.

COMMUNITY PLAN OBJECTIVES AND SPECIAL POLICIES: Utilizing the community plan goals established in Chapter 1, the following objectives and special policies shall be implemented as part of the community plan. Objectives represent desired ends or results. Policies are to be implemented as the means to achieve objectives. Chapter 7 identifies specific means to implement the policies.

- θ **Land Use and Economic Development Goal:** Maintain the long term economic health and stability of the plan area by providing a diverse mix of commercial, recreational and public service land uses in five separate land use districts which serve both residents and tourists.

Land Use Objectives:

1. Discourage perpetuating the strip commercial land use pattern and appearance. Designate specific land use districts within the community plan as priority areas to accommodate additional commercial development. Provide appropriate land use opportunities to realize the intent and theme of each district.
2. Encourage the following uses to locate or remain within the community plan: indoor and outdoor recreation, a full-service community market, a community meeting facility, California Highway Patrol offices, day care or preschool facility, a full-service post office and a bed and breakfast inn.
3. Discourage additional service stations, convenience retail stores, car sales lots and outdoor retail sales along U.S. 50.
4. Encourage the USDA Forest Service and other participating partners to develop a permanent visitors' center in the community plan area.
5. Provide opportunities to improve the connections between the community plan area and the adjacent Tahoe Paradise Golf Course.
6. Provide opportunities to transfer existing commercial land uses which are located near the community plan area to relocate into the community plan area.

Economic Development Objectives:

1. From plan adoption until December 31, 1996, an additional 15,400 square feet of commercial floor may be allocated within the Community Plan Area. Incentive programs should assign priority to commercial development projects which meet community plan objectives.

Policy: Projects seeking allocations of additional commercial floor area within the plan area shall be subject to the plan's Commercial Floor Area Allocation Procedures (Appendix C).[§]

Policy: Allocation procedure B.1 (Location Within Priority Land Use Districts) established by the Commercial Floor Area Allocation Procedures shall not be revised or amended for at least two years from the date of plan adoption.

Policy: In the event that additional commercial floor area is allocated to the plan area, it shall be distributed to each priority group identified in Appendix C in the same proportions as the existing allocations.

2. From Community Plan adoption until December 31, 1996, provide other incentives such as 10 tourist accommodation bonus units and additional land coverage to encourage area-wide improvements. Incentive programs should assign priority to commercial development projects which meet community plan objectives.

Policy: Projects seeking allocations of additional tourist accommodation units within the plan area shall be designed consistent with the Community Plan Design Review Guidelines (Appendix A).

3. Encourage land uses, development projects and activities that will enhance the "year round" economy.

4. A special events area for arts and crafts shows, seasonal sales, and other similar events should be established.

Policy: the preferred temporary events site is the proposed community park and ride facility. It should be designed in such a manner that it can accommodate both uses.

- θ **Community Design Goal:** Implement a comprehensive community design program which will improve the visual quality of the commercial area and help Meyers establish a sense of permanence.

Objectives:

1. Improve the physical appearance of all areas within the plan area. Encourage rehabilitation through the remodeling, upgrading, landscaping, and aesthetic improvements of buildings, other structures and signage.

Policy: All projects which expand or relocate units of use, including land coverage, shall implement or commit to a five year schedule to implement, the landscape improvements fronting its project area. The improvements

[§] Amended 12/20/95

are described in the Community Plan Design Review Guidelines (Appendix A) and are generally shown in Figure 4-2, Community Design Plan. This policy may be waived if the project is in an assessment or improvement district already committed to the improvements.

Policy: Outdoor retail sales and displays visible from U.S. 50 or California 89 shall be consistent with the outdoor display guidelines contained in the Design Review Guidelines. Outdoor storage of items to be sold or rented (not sales displays) shall not be visible from U.S. 50 or California 89.

Policy: Land uses in the U.S. 50 non-operational right-of-way which may be acquired by adjoining property owners shall be limited to parking, decks, paths, signage, landscaping, lighting and water quality control facilities.

2. Develop and implement design review guidelines unique to the Meyers area which reinforce the community design goal and established the “historic Meyers” architectural design theme.

Policy: All projects shall be consistent with applicable sections of the Community Design Plan and Design Review Guidelines (Appendix A). Appropriate guidelines may be added as conditions of approval.

3. Develop and implement substitute signs standards to TRPA and El Dorado County sign standards which provide opportunities for visible signage for all uses. Include recommendations for appropriate media and materials. Recognize and respond to the constraints created by the unusually wide U.S. 50 right-of-way and potential snow depths.

Policy: Signage shall be subject to the standards established in the Community Plan Substitute Sign Standards (Appendix B).

4. Implement a cooperative sign reduction and consolidation program with Caltrans for signs located within U.S. 50 and California 89 rights-of-way. Remove, reduce and consolidate highway informational and directional signage wherever possible.
5. Participate in the south shore’s Art in Public Places program. Encourage placement of public art within the community plan area.
6. Encourage scenic resource and related community design improvements within the U.S. 50 corridor through implementation of the Pat Lowe Bike Trail.
7. In consultation with Caltrans, revise current snow plowing and storage operations to improve visibility of adjacent businesses, reduce signage needs and maintain landscape plantings.

PERMISSIBLE USES: Pursuant to Chapter 18 PERMISSIBLE USES, the following primary uses may be permitted within all or a portion of the Community Plan Area. The list indicates if the use is allowed (A) or must be considered under the provisions for a special use (S). Existing uses not listed shall be considered nonconforming uses within the Community Plan Area. The establishment of new uses not listed shall be prohibited within the Community Plan Area.

θ **Yank's Station Land Use District (Special Area #1):** The following list of permissible uses is applicable in the Yank's Station land use district.

Residential Employee housing (S) ^{§§}, nursing and personal care[§] (S), and multiple person dwelling (S).

Tourist Accommodation Bed and breakfast facilities^{§§} (S), hotel, motel, and other transient dwelling units (S), time sharing (hotel/motel design)^{§§} (S), and time sharing (residential design) ^{§§} (S).

Commercial

A. Retail
Building materials and hardware (A), eating and drinking places (A), food and beverage retail sales (A), Food and kindred products^{§§} (S) furniture, home furnishings and equipment (A), general merchandise stores (A), mail order and vending (S), nursery (A), outdoor retail sales (S), and service stations (S).

B. Entertainment
Amusement and recreation services (A), privately owned assembly and entertainment (S), and outdoor amusements (S).

C. Services
Animal husbandry services (A), auto repair and service (S), broadcasting studios (A), business support services (A), contract construction services (S), financial services (A), health care services (A), personal services (A), professional offices (A), repair services (S), schools-business and vocational (S), and secondary storage (S)[§].

D. Light Industrial
(S), printing and publishing (S), and small scale manufacturing (S).

E. Wholesale/Storage
Warehousing^{§§§}(S), Secondary Storage^{§§§} (S) and wholesale and distribution (S).

[§] Amended 08/26/98

^{§§} Amended 12/20/95

^{§§§} Amended 12/20/95 Permissible on parcels fronting Santa Fe Road only.

Public Service

A. General

Churches (S), collection stations (S), cultural facilities[§] (S), day care centers and pre-schools (A), government offices (A), hospitals (S), local assembly and entertainment (S), local post office[§] (S), local public health and safety facilities (S), membership organizations (A), publicly owned assembly and entertainment (S), public utility centers (S), regional public health and safety facilities (S), schools and colleges[§] (S), schools (K-12)[§] (S), and social service organizations (A).

B. Linear Public Facilities

Pipelines and power transmission (S), transit stations and terminals[§] (S), transportation routes (S), and transmission and receiving facilities (S).

Recreation

Cross country ski courses (A), day use areas (A), outdoor recreation concessions (S), participant sports facilities (S), recreation centers (S), riding and hiking trails (A), sport assembly (S), and visitor information centers[§] (S).

Resource Management

A. Timber Management

Reforestation (A), sanitation salvage cut (A), special cut (A), thinning (A), and timber stand improvement (A).

B. Wildlife and Fish

Early successional stage vegetation management (A), nonstructural fish habitat management (A), nonstructural wildlife habitat management (A), structural fish habitat management (A), and structural wildlife habitat management (A).

C. Open Space

Allowed in all areas of the region.

D. Vegetation Protection

Fire detection and suppression (A), fuels treatment/management (A), insect and disease suppression (A), sensitive plant management (A), and uncommon plant community management (A).

E. Watershed Improvements

Erosion control (A), runoff control (A), and stream environment zone restoration (A).

[§] Amended 12/20/95 (Maintenance facilities are not allowed within any new transit facility)

θ **Lake Valley Land Use District (Special Area #2):** The following list of permissible uses is applicable in the Lake Valley land use district.

Residential	Employee housing [§] (S) and single family dwelling (S).
Tourist Accommodation	Bed and breakfast facilities (A).
Commercial	Amusement and recreation services (S), outdoor retail sales (S), personal services (A), privately owned assembly and entertainment (S), and outdoor amusement (S).
Public Service	A. <u>General</u> Churches (S), cultural facilities (A), day care centers and pre-schools (A), government offices (A), hospitals (S), local assembly and entertainment (A), local post offices (S), local public health and safety facilities (S), membership organizations (A), publicly owned assembly and entertainment (S), public utility centers (S), regional public health and safety facilities (S), and social service organizations (A). B. <u>Linear Public Facilities</u> Pipelines and power transmission (S), transit stations and terminals [§] (S), transportation routes (S), and transmission and receiving facilities (S).
Recreation	Cross country ski courses (A), day use areas (A), outdoor recreation concessions (A), participant sports facilities (S), recreation centers (A), riding and hiking trails (A), and visitor information centers [§] (S).
Resource Management	Same as Special Area #1

θ **West Meyers Land Use District (Special Area #3):** The following list of permissible uses is applicable in the West Meyers land use district.

Residential	Employee housing (S), multiple family dwelling (S), and single family dwelling [§] (S).
Tourist Accommodation	Bed and breakfast facilities [§] (S).
Commercial	A. <u>Retail</u> Building and materials and hardware (S),

[§] Amended 12/20/95 (Maintenance facilities are not allowed within any new transit facility)

eating and drinking places (A), food and beverage retail sales (A), furniture, home furnishings and equipment (A), general merchandise stores (A), mail order and vending[§] (S), nursery (S), and service stations (S).

B. Entertainment

Amusements and recreation services (A), privately owned assembly and entertainment (S), and other outdoor amusements (S).

C. Services

Animal husbandry services (S), broadcasting studios (A), business support services (A), financial services (A), health care services (A), laundries and dry cleaning[§] (S), personal services (A), professional offices (A), repair services (S), and schools-business and vocational (S).

D. Light Industrial

Food and kindred products[§] (S), and printing and publishing (S).

E. Wholesale/Storage

Warehousing (S) and wholesale and distribution (S).

Public Service

A. General

Churches (S), collection stations (S), cultural facilities (A), day care centers and pre-schools (A), Schools-Business and Vocational[§] (S) government offices (A), local assembly and entertainment (S), local post offices (A), local public health and safety facilities (S), membership organizations (A), publicly owned assembly and entertainment (S), public utility centers (S), regional public health and safety facilities (S), and social service organizations [§] (S).

B. Linear Public Facilities

Pipelines and power transmission (S), transit stations and terminals (A), transportation routes (S), and transmission and receiving facilities (S).

Recreation

Cross country ski courses (A), day use areas (A), outdoor recreation concessions (A), participant sports facilities (A), recreation centers (A), riding and hiking trails (A), visitor information centers (A).

[§] Amended 12/20/95

Resource Management Same as Special Area #1.

θ **Industrial Tract Land Use District (Special Area #4):** The following list of permissible uses is applicable in the Industrial Tract land use district.

Commercial

- A. Retail
Auto, mobile home and vehicle dealers (S), building and materials and hardware (A), eating and drinking places (A), food and beverage retail sales (S), furniture, home furnishings and equipment[§] (S), general merchandise stores (S), mail order and vending (A), nursery (A), and outdoor retail sales[§] (S).
- B. Services
Animal husbandry services (A), auto repair and service (S), broadcasting studios (A), business support services (A), contract construction services (A), financial services (S), health care services (S), laundries and dry cleaning (S), personal services[§] (S), professional offices (S), repair services (A), sales lots (S), and secondary storage (S).
- C. Light Industrial
Food and kindred products (A), fuel and ice dealers (A), industrial services (S), printing and publishing (A), recycling and scrap (S), and small scale manufacturing (S).
- D. Wholesale/Storage
Storage yards (S), vehicle and freight terminals (S), vehicle storage and parking (A), warehousing (A) and wholesale and distribution (A).

Public Service

- A. General
Collection stations (A), cultural facilities (S), day care centers and pre-schools (S), government offices (S), local assembly and entertainment (S), local public health and safety facilities (A), public utility centers (S), and regional public health and safety facilities (S).
- B. Linear Public Facilities
Pipelines and power transmission (S), transit stations and terminals (A), transportation routes (S), and transmission and receiving facilities (S).

[§] Amended 12/20/95

Recreation Cross country ski courses (A), outdoor recreation concessions (S), participant sports facilities (S), riding and hiking trails (A), and rural sports (S).

Resource Management Same as Special Area #1.

θ **Upper Truckee River Land Use District (Special Area #5):** The following list of permissible uses is applicable in the Upper Truckee River land use district.

Residential Single family dwelling (A).

Tourist Accommodation Bed and breakfast facilities (S).

Public Service

A. General
Cultural facilities (S), day care centers and pre-schools (S), local public health and safety facilities (S) and Visitors Information Center^{§§}. (S)

B. Linear Public Facilities
Pipelines and power transmission (S), transit stations and terminals^{§§} (S), transportation routes (S), and transmission and receiving facilities (S).

Recreation Cross country ski courses (S), day use areas (S), riding and hiking trails^{§§} (S), and visitor information centers (S).

Resource Management Same as Special Area #1.

MAXIMUM DENSITIES: Pursuant to Chapter 21 DENSITY, the following list established the maximum allowable densities that may be permitted for any parcel located within the Community Plan Area. The actual development permitted may be further limited by transfer of development rights limitations, residential density incentive programs, special use determinations, allocation limitations and general site development standards.

USE	MAXIMUM DENSITY
Residential	
Single Family Dwelling	1 unit per parcel
Multiple Family Dwelling	15 units per acre
Multiple Person Dwelling	25 people per acre
Nursing and Personal Care [§]	25 persons per acre
Employee Housing ^{§§}	15 units per acre

^{§§} Amended 12/20/95

[§] Amended 08/26/98

Tourist Accommodation

Bed and Breakfast	10 units per acre
Hotel, Motel and other Transient Units	
• with less than 10% of units with kitchens	40 units per acre
• with 10% or more units with kitchens	15 units per acre
Time Sharing (Hotel/Motel Design) ^{§§}	40 units per acre
Time Sharing (Residential Design) ^{§§}	15 units per acre

RESIDENTIAL BONUS UNITS: Pursuant to Chapter 35, the maximum number of residential bonus units which may be permitted for this Community Plan Area is 0 units.

TOURIST ACCOMMODATION BONUS UNITS: Pursuant to Chapter 35, the maximum number of tourist accommodation bonus units which may be permitted for this Community Plan Area is 10 units.

ADDITIONAL DEVELOPED OUTDOOR RECREATION: The following are the PAOT targets and limits for additional developed recreation facilities specified in Chapter 13 to be located within this Community Plan Area. These and other recreation target projects are addressed in the TRPA Five-Year Recreation Program pursuant to Chapter 33 Allocation of Development. The following additional capacities allowed are measured in persons at one time:

SUMMER DAY USES **0 PAOT** WINTER DAY USE **0 PAOT** OVERNIGHT USES **0 PAOT**

COMMERCIAL FLOOR AREA ALLOCATION: Pursuant to Chapter 33, the maximum amount of commercial floor area which may be allocated for additional development in the Community Plan Area is 25,400 square feet. [§]

MAXIMUM CUMULATIVE NOISE LEVEL: The maximum cumulative noise equivalent level for the Community Plan Area is 65 CNEL. The following noise performance standards shall also be met.

Noise Level Descriptor	Daytime 7 a.m. – 7 p.m.		Nighttime 7 p.m. – 7 a.m.	
	Special Areas #1, #2, #3, #5	Special Area #4	Special Areas #1, #2, #3, #5	Special Area #4
Hourly Leq db	55	60	45	50
Maximum Level, db	75	75	60	70

Notes:

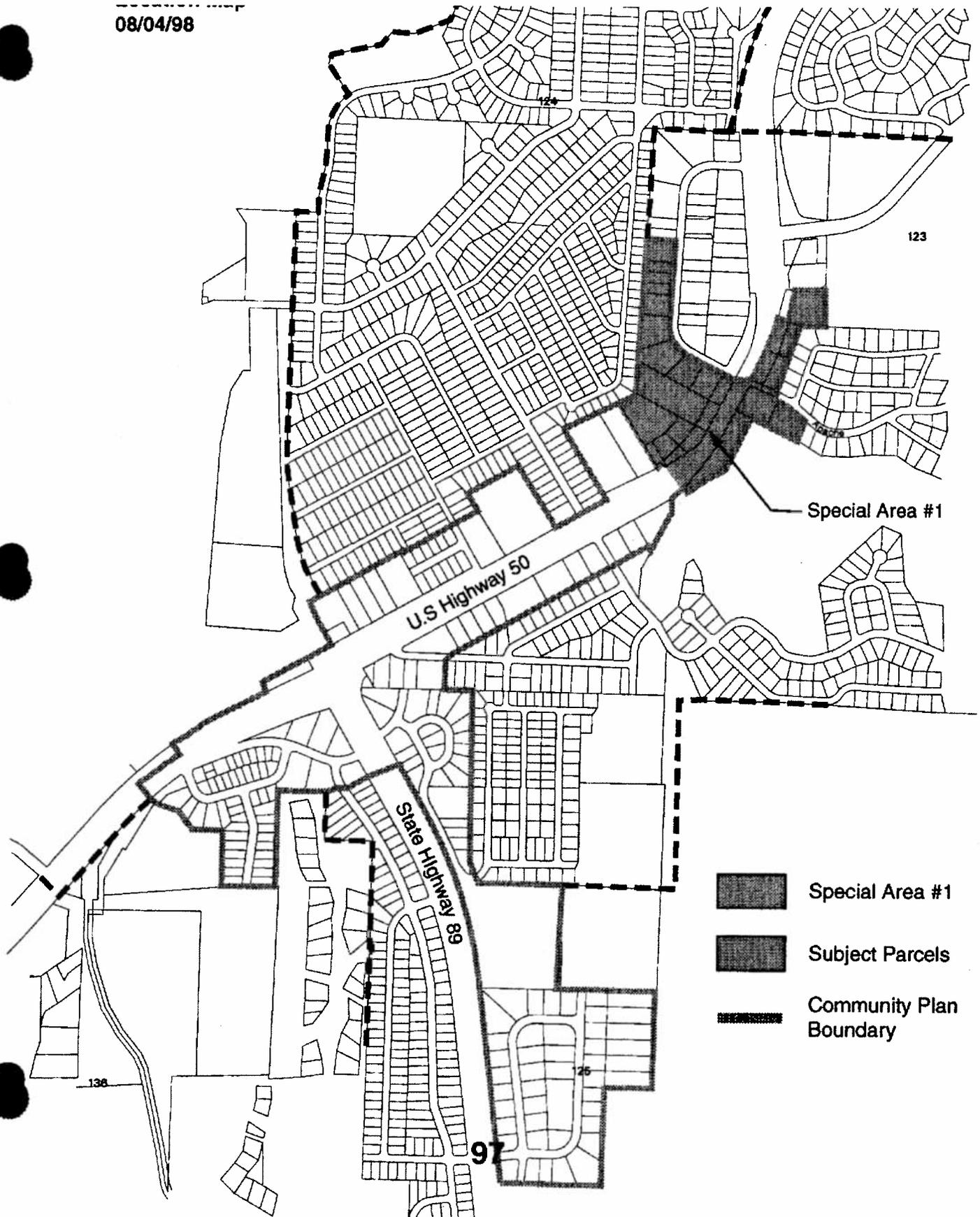
1. The hourly Leq and the maximum level noise standards specified above shall be lowered by 5db for simple tone noises, noises consisting primarily of speech or music, or for recurring impulsive noises.

^{§§} Amended 12/20/95

[§] Amended 05/14/98.

2. The standards shall be applied at the plan area boundary and at the property line of any residential use within the community plan area.
3. In addition to the performance standards, stationary or industrial noise sources which may impact land uses in adjacent plan area(s) must comply with the Plan Area Statement CNEL standards of the adjacent plan area(s).

08/04/98



Chapter 3

TRANSPORTATION AND CIRCULATION ELEMENT

The transportation and circulation element is a supplement to TRPA's Regional Transportation Plan/Air Quality Plan (RTP/AQP) and to the Transportation Element of El Dorado County's General Plan. Consistent with these plans, this chapter lists the specific policies applicable to the community plan and describes the improvements necessary to implement it. Specific capital improvements and schedules are found in Chapter 7, Implementation.

The plan's transportation goal is to reduce dependence on the private auto mobile through a series of incremental actions incrementally. This goal will be accomplished by clustering land uses to eliminate short trips, providing convenience goods and services to nearby residents, thus diverting trips destined for South Lake Tahoe and providing bicycle and pedestrian paths to encourage alternative transportation.

A. EXISTING TRANSPORTATION SYSTEM

Streets and Highways

U.S. Highway 50 and California State Route 89 serve as the primary highway links in and through the community plan area. U.S. 50 enters the Tahoe Basin a short distance away over Echo Summit. It provides the main point of access from the west slope and central valley population centers. U.S. 50 is a two lane highway with a continuous, two-way center turn lane between California 89 and Santa Fe Street. Pioneer Trail, classified as a minor arterial roadway, intersects U.S. 50 immediately north of the community plan boundary. It is an important intersection in the area connecting Meyers to South Lake Tahoe and Stateline. Eight collector streets serve the surrounding residential neighborhoods and intersect either U.S. 50 or California 89. California 89 also provides entry into the Tahoe Basin from Hope Valley and outlying communities in Alpine County.

U.S. 50 and California 89 are designated as state scenic highways through the plan area. Both are designated as TRPA regional scenic highway corridors. The TRPA designation includes all lands visible within one thousand feet from the edge of right-of-way for both highways.

Perhaps the most significant physical feature in Meyers is the U.S. 50 right-of-way. Originally planned to contain a four lane, limited access freeway and frontage roads, it contains only two travel lanes and a center turn lane. It varies in width from approximately 100 feet near Yanks' Station to 280 feet in West Meyers. Undeveloped land within the right-of-way is mostly disturbed and contains numerous unpermitted signs, parking and outdoor retail displays. It has adversely affected the community in terms of circulation, water quality, site planning, architectural design and business activity. See Figure 3-1.

Transit Service

El Dorado County presently operates limited transit service to the plan area. It provides route deviation, on-demand transit service known as Bus Plus, which is operated by

Area Transit Management under contract to the County. It operates twenty four hour a day and serves the unincorporated area of El Dorado County in the Meyers area. Persons who wish to ride Bus Plus call the service in advance to arrange to be picked up. It is coordinated with South Tahoe Area Ground Express (STAGE) transit service within the City of South Lake Tahoe and Stateline. Parking Facilities

Most existing uses have on-site parking although little, if any of it, is organized or marked. Several uses along U.S. 50 in the West Meyers area use parking within the adjacent non-operational right-of-way. In most cases, the right-of-way parking exists without permits or adequate water quality control measures. With the exception of uses which use the right-of-way for parking, the supply of parking is generally adequate. Right-of-way parking is being improved as part of constructing the Pat Lowe Memorial Bike Trail.

Pedestrian/Bicycle Facilities

A Class I bicycle trail, known as the Pat Lowe Memorial Bike Trail, runs along the south side of U.S. 50 from Pioneer Trail to California 89. Phase II of the project along the north side of U.S. 50 is scheduled for construction during 1994. An existing dirt trail runs along the east side of California 89 connecting the plan area with residential neighbor hoods of the Upper Lake Valley (Christmas Valley). The trail is used by equestrian riders as well as off-road bicyclists and pedestrians. Other informal dirt paths connect the plan area to adjacent residential neighborhoods or nearby open space.

Existing Traffic Conditions

There are two distinct sets of traffic conditions in Meyers: free-flowing and congested. Most of the time vehicle traffic on U.S. 50, California 89 and intersecting side streets flows freely. This generally occurs during weekday periods throughout most of the year except for holidays which fall or are observed on a weekday.

Congested traffic conditions generally occur during weekends and holidays. The normal pattern involves visitors entering the basin on Fridays and Saturdays and leaving the basin on Sundays or on weekday holidays. It causes severe backups and queues on U.S. 50 for motorists travelling back and forth over Echo Summit. During winter snowstorms the situation can be easily compounded by chain-up and snow removal procedures. Congestion caused by U.S. 50 traffic exiting the basin produces backups at intersections within the plan area including California 89 and most side streets. This is especially true for vehicles (generally local residents) attempting to make left turns. During periods of particularly bad congestion, the backup of traffic on U.S. 50 traffic leaving the basin can extend all the way into the City of South Lake Tahoe. When this occurs, traffic in Meyers can become gridlocked.

1990 Caltrans traffic counts on U.S. 50 through Meyers indicate peak hour traffic (summer weekend) of 2,650 vehicles at the California 89 intersection and 1,950 vehicles at the Pioneer Trail intersection. Average Annual Daily Traffic (AADT) at the same points was estimated at 17,300 and 13,600, respectively.

In 1981, TRPA estimated the Vehicle Miles of Travel on a peak summer day within the community plan area at 9,974. By 1987, region wide VMT in creased by approximately 10.2%. Reducing VMT is an important component of the plan. It is more fully discussed in Chapter 4.

The 1992 Lake Tahoe Airport Master Plan Draft EIR/EIS estimated the level of service at

the signalized U.S. 50/Pioneer Trail intersection to be at level "C", based on a volume/capacity ratio of 0.71. Level "C" represents stable, free flowing conditions. Although unsignalized, the U.S. 50/California 89 intersection is often congested on peak weekends, and likely exceeds level "D".

B. COMMUNITY PLAN TRANSPORTATION OBJECTIVES AND SPECIAL POLICIES

Utilizing the community plan goals established in Chapter 1, the following objectives and special policies shall be implemented as part of the community plan. Objectives represent desired ends or results. Policies are implemented as the means to achieve objectives.

Transportation and Circulation Goal:

Redevelop the transportation system within the community plan area to reduce reliance on the private automobile, improve circulation and provide opportunities to experience Meyers as a pedestrian or cyclist.

Objectives and Special Policies:

1. Implement transportation and circulation improvements in Meyers to improve existing conditions and achieve the plan's transportation and circulation goal.

Policy: El Dorado County, TRPA and Caltrans shall implement transportation improvements called for in the Regional Transportation Plan/Air Quality Plan. The improvements generally include the Pat Lowe Memorial Bike Trail along U.S. 50 and expansion of El Dorado County transit service.

Policy: All uses within the community plan with 50 or more employees shall implement transportation demand management strategies (TDM) including, but not limited to, employer-based trip reduction programs, ride sharing programs and transit incentives.

2. Identify opportunities to reduce traffic speeds through Meyers without adversely affecting air quality. Consider using right-of-way improvements and traffic controls to achieve the objective.

Policy: Maintain Level of Service of "D" or better at U.S. 50/Pioneer Trail and U.S. 50/California 89 intersections consistent with the RTP/AQP.

3. Encourage El Dorado County, transit providers and recreation area shuttles to increase transit service to and from Meyers. Provide high quality transit stops and facilities which encourage ridership and are designed consistent with the Community Plan Design Review Guidelines.

Policy: Strategically locate transit stops in each land use district as the need and demand is demonstrated.

Policy: Transit shelters shall be provided at all transit stops. Transit shelters shall be designed consistent with the Community Plan Design Review Guidelines.

4. Provide park and ride opportunities in Meyers. Encourage the use of shared community parking areas which can serve the land use districts.

Policy: Develop a community park and ride facility within the community plan

area which can be served by transit and recreation area shuttles. The preferred site is the vacant State of California parcels adjacent to the Agricultural Inspection Station.

5. In consultation with Caltrans, TRPA and the California Highway Patrol cooperatively redesign the chain-up areas and modify chain-up procedures including short term truck parking, along U.S. 50. Consider using the park and ride facility adjacent to the Agricultural Inspection Station as an alternate chain-up area. Provide community input and assistance in developing and implementing the improvements.

Policy: Caltrans, the California Highway Patrol, TRPA and other interested parties shall develop an action plan to improve chain control operations which minimize adverse impacts to circulation and traffic flow and provide safe and efficient chain-up opportunities. This may include developing a designated chain-up area(s) along U.S. 50.

6. Encourage pedestrian and bicycle linkages between land uses. Accommodate pedestrians throughout the community plan area by providing safe, functional pathways.

Policy: Support development of the Pat Lowe Memorial Bike Trail.

7. Reduce the number of vehicular access points and other points of conflict along U.S. 50. Encourage the use of shared driveways along U.S. 50. Coordinate the access locations with the planned U.S. 50 center median.

Policy: As a condition of project approval, reduce or consolidate the number of access points along U.S. 50 to improve traffic flow and eliminate vehicle conflicts with pedestrians and bicyclists. This policy shall apply to uses with more than one U.S. 50 access and to adjoining uses with access points less than 100 feet apart where the site layouts will accommodate the combined access without major modification.

8. Recognize the status of U.S. 50 and California 89 as state and regionally-designated scenic highway corridors. Maintain and improve the scenic quality of the corridors and their viewsheds.

Policy: Transportation projects shall be designed consistent with the Community Design Plan and Design Review Guidelines.

Policy: Request Caltrans to implement a snow removal and snow haul program to remove plowed snow from in front of U.S. 50 businesses. Assist in developing alternate snow storage sites and funding sources.

9. Reduce the visual dominance of the U.S. 50 highway corridor through Meyers.

Policy: Request Caltrans to make available for sale or other means of transfer, excess right-of-way along U.S. 50 to adjacent property owners.

Policy: Develop a center lane landscaped median along U.S. 50 through the community plan area which provides access and visibility to adjoining land uses, and permits turn movements at cross street intersections.

C. COMMUNITY PLAN TRANSPORTATION AND CIRCULATION IMPROVEMENTS

The following improvements are necessary to implement the policies listed above. The improvements generally reflect or refine items listed in Volume III of the RTP/AQP. They are shown in Figure 3-1. Additional discussion of improvements, estimated costs and methods of implementation is located in Chapter 7, Implementation.

Streets and Highways

1. Consolidate Existing Driveways Along U.S. 50. Reduce and/or consolidate existing access points along U.S. 50 consistent with the access policy listed above under Objective #7. The policy establishes criteria to determine which driveways will be affected. Targeted areas of highest priority for consolidation are indicated on Figure 3-1. Construction of the Pat Lowe Memorial Bike Trail and individual projects will be the primary means of implementation.
2. Develop and Implement a Chain Control Operations Plan. In consultation with Caltrans and the Highway Patrol, prepare an action plan which improves chain control procedures along U.S. 50. The plan may include operational changes as well as site improvements to facilitate chain-up while maintaining traffic flow and turning movements. Figure 3-1 identifies the plan's preferred location for a designated chain-up area located west of the U.S. 50/California 89 intersection.
3. [§]Develop and Implement U.S. 50/California 89 Intersection Improvements. Caltrans and El Dorado County should cooperatively study the opportunities for intersection improvements to maintain or improve the level of service consistent with the level of service policy listed above under Objective #2. Possible improvements could include a traffic circle or traffic signal. Improvements should be installed when warranted.
4. Construct a Center Landscaped Median Along U.S. 50. In consultation with Caltrans, develop and construct an intermittent landscaped median in the existing, continuous two-way left turn lane of U.S. 50. The median will reduce the apparent width of the highway and reduce overall traffic speeds thereby improving vehicle and pedestrian safety. Vehicle turning pockets and associated stacking space are to be included as part of the project to serve cross street intersections and major driveways. The median should be planted with low maintenance native or adapted shrubs and ground cover. The plant palette should be selected in part to provide for necessary vehicle sight distances and visibility of roadside businesses. Snow storage and drainage facilities will be included to prevent roadway icing during winter. The project should include pedestrian crossings in certain locations to improve pedestrian safety. See Figure 4-2, Community Design Plan, for approximate median locations.

Transit Service

1. Expand El Dorado County Transit Service. Fixed route transit service should be provided to each land use district including the community park and ride

[§] Amended 12/20/95

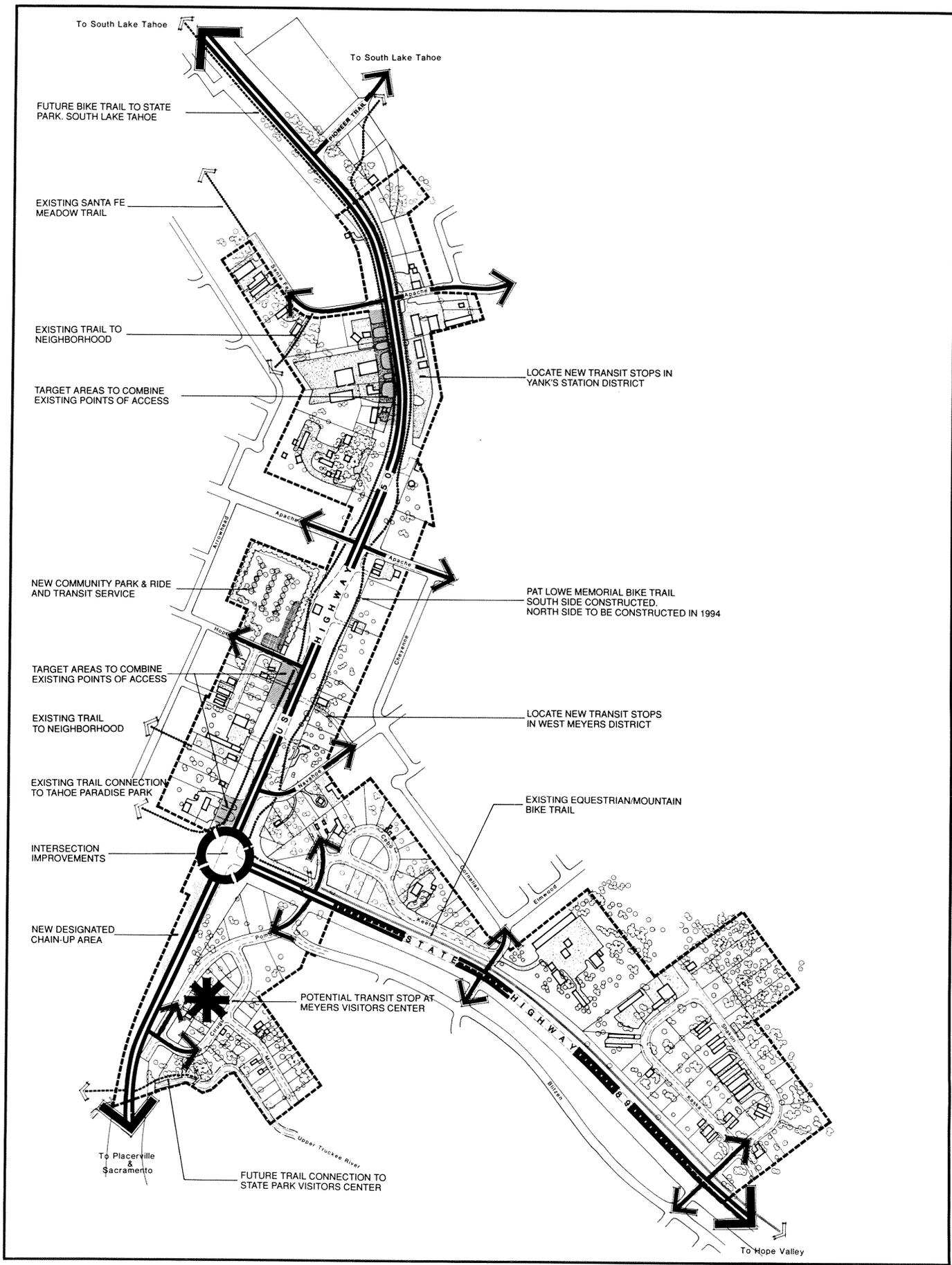
facility, and the City of South Lake Tahoe/Stateline. On demand service to the outlying residential and recreational uses should connect to the community plan area, preferably at the park and ride facility. Construct transit shelters at each transit stop throughout the plan area, including the park and ride facility.

Parking Facilities

1. Develop the Meyers Community Park and Ride. Develop a community park and ride facility within the plan area. The preferred site is the State of California parcels adjacent to the Agricultural Inspection Station. The facility's program of uses includes park and ride lot, transit and recreation shuttle stop, chain-up and rest area. Based on land coverage limitations and other site design factors, it is estimated to accommodate up to 150 vehicles. It can be designed to prohibit vehicles avoiding the nearby Agricultural Inspection Station by accessing off Hopi Street. If the preferred site is developed, the Department of Agriculture may decide to transfer control of the site to another agency such as the California Tahoe Conservancy.

θ Pedestrian/Bicycle Facilities

1. Complete the Pat Lowe Memorial Bike Trail. Complete the trail as planned on both sides of U.S. 50. Although located outside the plan area, the plan recommends extending the Pat Lowe Trail along U.S. 50 toward the Lake Valley State Recreation Area (Lake Tahoe Country Club) and South Lake Tahoe.
2. Develop Recreation Access Trailheads. Construct recreational trail heads as part of developing the Meyers Visitors Center and Washoe Meadows State Park. The trailheads would access public lands near the Upper Truckee River bridge.



Meyers Community Plan TRANSPORTATION AND CIRCULATION

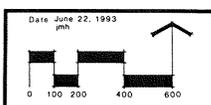


Figure 3-1. Transportation and Circulation

Chapter 4

ENVIRONMENTAL CONSERVATION ELEMENT

The environmental conservation element is a supplement to the Conservation Element of the TRPA Goal and Policies Plan. It contains the plan's environmental conservation and management strategy. It lists the required environmental improvement targets which the community plan will achieve. The plan recognizes and includes all environmental improvements implemented in the plan area since July 1, 1987. The conservation element contains updated land capability and stream environment zone mapping which is adopted as part of the community plan.

A. COMMUNITY PLAN ENVIRONMENTAL CONSERVATION OBJECTIVES AND SPECIAL POLICIES

Utilizing the community plan goals established in Chapter 1, the following objectives and special policies shall be implemented as part of the plan. Objectives represent desired ends or results. Policies are implemented as the means to achieve objectives.

0 Environmental Conservation Goal

Improve environmental values in Meyers as part of implementing the community plan.

Objectives and Special Policies:

1. Attain and maintain the environmental threshold carrying capacity targets and other environmental targets contained in the plan.

Policy: The plan recognizes and incorporates environmental improvement projects implemented in the plan area since July 1, 1987. Such projects include, but are not limited to, placing overhead utility lines underground along U.S. 50 and constructing one half of the Pat Lowe Memorial Bike Trail and related water quality improvements.

Policy: All projects, whether the proponent is a private or public entity, shall implement the applicable improvement projects identified herein.

Policy: Public agencies having jurisdiction within the plan area are requested to implement public improvement projects identified in the plan. The policy shall apply to plans and programs which may be contained in an agency's capital improvements program.

2. Identify and protect the unique Western juniper tree. Consider its potential as an interpretive resource.

Policy: All live Western juniper trees within the plan area which are not diseased shall be preserved.

3. Protect and enhance existing scenic views and vistas from the plan area.

4. Protect and enhance stream environment zones. Encourage property owners with natural drainage channels in closed conduits to restore them to an open system.

5. Take an active role in the planning and review of nearby projects which may impact Meyers.[§]

B. ENVIRONMENTAL IMPROVEMENT TARGETS

The following key environmental improvement targets shall be implemented as part of the community plan. Achieving the targets is an important consideration for approving community plans, including levels of additional growth. Development opportunities are directly tied to implementing the targets. See Chapter 7, Implementation for further discussion and project cost estimates.

θ Water Quality Targets

1. Install and maintain Best Management Practices (BMPs) on all developed or disturbed parcels consistent with the adopted Water Quality Management Plan for the Lake Tahoe Region and TRPA Code of Ordinances, Chapter 25.

Program Description: Install and maintain on-site Best Management Practices for storm water runoff and erosion control. BMPs may include, but not be limited to, rock-lined or other conveyance channels, detention/retention basins, catch basins and drop inlets, infiltration wells, paving and revegetation. Public rights-of-way where BMPs are needed are shown in Figure 4-1.

Implementation Strategy: Install as part of project construction; existing retrofit schedules are generally based on a previous project approval; implementing future BMPs shall be based on a schedule in accordance with terms of state or federal storm water discharge permit, or a TRPA-issued remedial action plan.

2. Restore approximately 1.5 acres of disturbed Stream Environment Zone (SEZ) within and adjacent to the plan area.

Program Description: Restore disturbed SEZ sites shown in Figure 4-1. Targeted sites generally include: Minal Street cul-de-sac (0.25 acres in area); Shell, Supply One and Chevron sliver parcels adjacent to U.S. 50 (total area 1.1 acres); and existing SEZ and channel behind Yank's Station (0.15 acres).

Implementation Strategy: El Dorado County, USDA Forest Service and private property owners will take a lead role depending on the project. Several public funding sources are available to implement projects. Conditions of project approval, including man-modified land capability challenge approvals may also be used to achieve the target. At least 50% of the targeted areas should be restored by 1997, with the remaining 50% completed by 2007.

θ Air Quality Targets

1. Reduce approximately 1,330 Vehicle Miles Travelled (VMT) on a peak summer day.

Program Description: Implement action element for Meyers area identified in TRPA's Regional Transportation Plan/Air Quality Plan, Chapter 3. Measures include expanded public transit service to Meyers and surrounding areas

[§] Amended 12/20/95

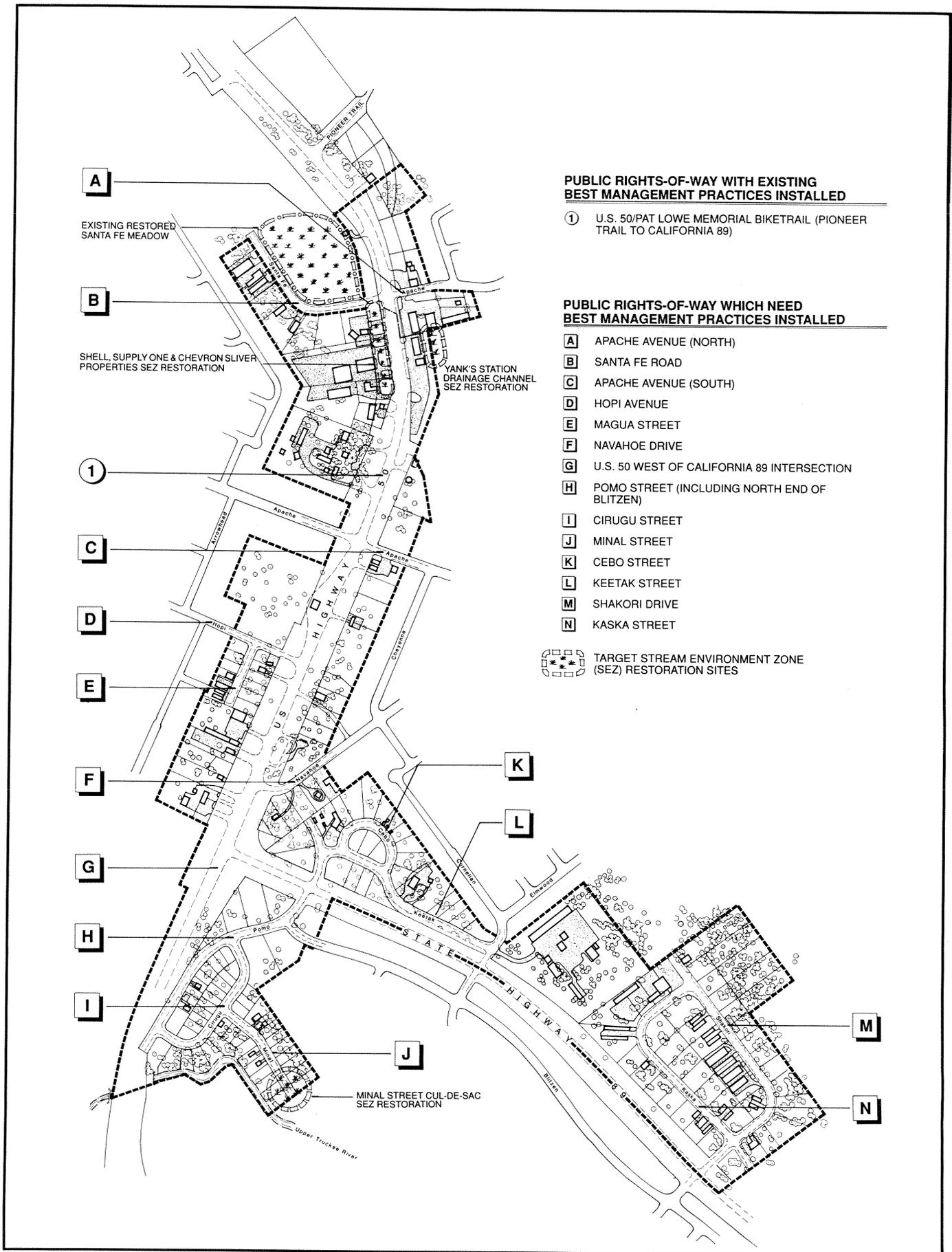


Figure 4-1. Water Quality Improvements

(includes expanded service area, service hours and reduced headways) and completion of the Pat Lowe Memorial Bike Trail. Additional target measures include: clustering land uses to reduce single-purpose vehicle trips; providing convenience goods and services within the plan area to divert trips by local residents otherwise destined for South Lake Tahoe (i.e., longer vehicle trip); implementing employer based trip reduction program; and utilizing the interim and the proposed permanent Meyers Visitors Center to provide information and make overnight reservations.

Implementation Strategy: Conditions of project approval; State and federal transportation funding programs; TRPA air quality mitigation funds; and implementing employer based trip reduction/ride sharing program. The South Shore Transportation Management Association (TMA) will take an active role in transit service expansion.

2. Maintain a Level of Service "D" or better at key intersections in and adjacent to the plan area.

Program Description: Key intersections include U.S. 50 and Pioneer Trail and U.S. 50 and California 89. Program measures include reducing and consolidating private driveways along U.S. 50 (Chapter 3, objective #7); installing right turn pockets at major driveways or side streets as warranted; installing landscaped center median to control turning movements; potential signalization or other traffic control measure (e.g., traffic circle) at U.S. 50/California 89 intersection.

Implementation Strategy: Conditions of project approval; State and federal transportation funding programs; and TRPA air quality mitigation funds.

θ **Scenic Resources Targets**

1. Attain and maintain a threshold travel route rating of 16 for Roadway Unit #36 (U.S. 50 strip) within the plan area. Maintain a threshold scenic quality rating of 7 for scenic resource #36-1.

Program Description: Roadway Unit #36: Improve the visual quality of the built environment consistent with the general recommendations for architecture and site planning found in TRPA's Scenic Quality Improvement Program (SQIP). Specific recommendations are contained in the Community Design Program (includes design review guidelines, substitute sign standards and community design plan). See Figure 4-2. They include: architectural improvements; screening of parked vehicles and outdoor storage; adding landscaping; screening or relocating outdoor sales displays; reduction of advertising signage; reduction and consolidation of highway directional signage; and development of highway corridor improvements.

Implementation Strategy: Install individual site improvements as conditions of project approval. Such improvements may be funded, in part, through revolving loans established at area banks; U.S. 50 public improvements installed as through a community capital improvement program. Signs will be brought into conformance with adopted regional and community plan sign standards.

Note: The plan area contains minor portions of Roadway Units #37 (Echo Summit) and #38 (Upper Truckee River). Roadway Unit #37 has an existing

travel route rating of 26 and is located along U.S. 50 west of the intersection with California 89. Roadway Unit #38 has an existing travel route rating of 18 and is located along California 89. Both units are considered to attain the threshold, therefore, no additional improvements are proposed by the plan. There are no mapped scenic resources within the plan area in either Roadway Unit #37 or #38.

2. Convert existing aerial utilities to underground systems.

Program Description: Convert existing overhead utility lines located along U.S. 50 between the California 89 intersection and the Upper Truckee River to underground systems.

Implementation Strategy: El Dorado County designates the area containing the lines as an undergrounding district. Utility line owners and users subsequently convert the aerial systems to underground systems pursuant to Rule 20 of the California Public Utilities Code. Service connections are placed underground by property owners using the utilities.

θ **Community Design Target**

Target: Implement Community design criteria consistent with TRPA's adopted Community Design threshold. The regional threshold generally requires the design elements of all new, remodeled or redeveloping uses be compatible with the natural, scenic, and recreational values of the Region.

Program Description: The community plan design program is an integrated program intended to improve the visual quality of the built environment and foster a sense of place in Meyers. It includes a Community Design Plan (Figure 4-2), Design Review Guidelines (Appendix A), and substitute sign standards (Appendix B). Refer to Chapter 2, Community Design Special Policies.

Implementation Strategy: Install individual site improvements as conditions of project approval. Such improvements may be funded, in part, through revolving loans established at area banks; U.S. 50 public improvements installed as through a community capital improvement program. Signs will be brought into conformance with adopted regional and community plan sign standards.

θ **Soil Conservation Targets**

1. Reduce existing land coverage within the plan area.

Program Description: Remove approximately 5,000 square feet of existing excess hard and soft land coverage from lands within the plan area. Permanently restore the lands from which the coverage was removed. Based on the estimated value of future construction associated with the plan, restore a minimum of 5,000 square feet of either hard or soft land coverage. Priority target sites include SEZ lands within the plan area which are overcovered. Secondary priority sites include lands of other capability class which are overcovered.

Implementation Strategy: Conditions of project approval on individual projects. Restoration of SEZ lands containing land coverage.

2. Restore disturbed lands within the plan area.

Program Description: Restore approximately 5 acres of existing disturbed lands within the plan area. Priority target sites include areas within the U.S. 50 right of way which are associated with the Pat Lowe Memorial Bike Trail. Other priority sites include disturbed SEZ lands.

Implementation Strategy: Disturbed lands will be restored as part of constructing the bike trail project.

θ **Noise Target**

Target: Attain and maintain applicable noise standards established for the plan area and listed in the Plan Area Statement.

Program Description: Identify noise sources which exceed the threshold standard and/or are producing significant adverse impacts. Attain and maintain noise standards through application of mitigation measures.

Implementation Strategy: Implement Chapter 23 of TRPA Code of Ordinances. Additional short-term noise performance standards are established for certain uses in the Plan Area Statement and are implemented as conditions of project approval. Noise mitigation techniques include, but are not limited to, relocation of use, incorporating site design factors such as noise barriers, hours of operation, use of structures to house noise sources, landscaping and setback of structures or noise-generating uses.

θ **Vegetation Preservation Target**

Target: Preserve unique vegetation.

Program Description: Preserve all existing live Western juniper trees which are not diseased in the plan area. Use the tree's image as a symbol of the Meyers area.

Implementation Strategy: Identify existing tree locations. Site development away from the trees. Incorporate trees into the overall site design. Avoid damage during construction, operation and maintenance activities.

θ **Outdoor Recreation Target**

Target: Provide high quality recreational opportunities.

Program Description: Develop permanent Meyers Visitors Center. Instantaneous capacity is expected to be approximately 150 persons at one time (PAOTs). The preferred site is near the corner of U.S. 50 and California 89.

Implementation Strategy: Allocate Summer Day Use PAOTS to the Visitors Center from the Regional Plan allocation pool. Develop and operate the visitors center through the interagency steering committee.

GATEWAY ENTRY FEATURE

- MEYERS SIGN, WESTERN JUNIPER TREES AND BOULDERS

REMOVE AND/OR COMBINE EXISTING DIRECTIONAL SIGNS IN RIGHT-OF-WAY

IMPROVE BUFFER ADJACENT TO RESIDENTIAL USES WITH SOUND WALLS AND/OR LANDSCAPING

COMMUNITY PARK & RIDE FACILITY

DESIGN PROGRAM:

- SECURE COMMUNITY PARKING
- TRANSIT AND RECREATION SHUTTLES
- REST STOP AND CHAIN-UP AREA
- WELL SCREENED FROM RESIDENTIAL NEIGHBORHOOD
- ARTS AND CRAFTS FAIR SITE

REMOVE AND/OR COMBINE AT LEAST 50% OF EXISTING DIRECTIONAL SIGNS IN RIGHT-OF-WAY IN SHADED AREAS

GATEWAY ENTRY FEATURE

- MEYERS SIGN, WESTERN JUNIPER TREES AND BOULDERS

PERMANENT MEYERS VISITORS CENTER PREFERRED SITE

EXISTING EQUESTRIAN/MOUNTAIN BIKE TRAIL

UPPER TRUCKEE RIVER DISTRICT

DESIGN STRATEGIES:

- UNDERGROUND EXISTING OVERHEAD UTILITY LINES
- PROVIDE INFORMAL ACCESS TO RIVER
- INSTALL ARCHITECTURAL IMPROVEMENTS
- BRING SIGNS INTO CONFORMANCE
- RETROFIT EXISTING EXTERIOR LIGHTING WITH RECOMMENDED FIXTURES
- DEVELOP DEDICATED CHAIN-UP AREA

WEST MEYERS DISTRICT

DESIGN STRATEGIES:

- INSTALL ARCHITECTURAL IMPROVEMENTS
- LANDSCAPE PARKING AREAS AND DISTURBED AREAS
- UNDERGROUND EXISTING UTILITY LINES CROSSING U.S. 50
- BRING SIGNS INTO CONFORMANCE
- RETROFIT EXISTING EXTERIOR LIGHTING WITH RECOMMENDED FIXTURES
- IMPROVE OUTDOOR RETAIL SALES DISPLAYS TO MEET PLAN POLICIES AND GUIDELINES
- INSTALL TWO TRANSIT STOPS AND SHELTERS
- ACQUIRE U.S. 50 EXCESS RIGHT-OF-WAY LANDS

INTERIM VISITORS CENTER

YANK'S STATION DISTRICT

DESIGN STRATEGIES:

- INSTALL ARCHITECTURAL IMPROVEMENTS
- LANDSCAPE PARKING AREAS AND OTHER LARGE PAVED AREAS
- BRING SIGNS INTO CONFORMANCE
- RETROFIT EXISTING EXTERIOR LIGHTING WITH RECOMMENDED FIXTURES
- IMPROVE OUTDOOR RETAIL SALES DISPLAYS TO MEET PLAN POLICIES AND GUIDELINES
- INSTALL TWO TRANSIT STOPS AND SHELTERS

HIGHWAY LANDSCAPED MEDIAN

DESIGN STRATEGIES:

- REDUCE VISUAL DOMINANCE OF HIGHWAY WITH LANDSCAPING
- INSTALL INTERMITTENT MEDIAN IN GENERAL LOCATIONS SHOWN
- INSTALL LANDSCAPING AND LIMITED PARKING AT AGRICULTURE INSPECTION STATION
- PROVIDE LEFT TURN OPPORTUNITIES AND STACKING SPACE AT ROAD INTERSECTIONS AND FOR MAJOR DIVEWAYS
- MAINTAIN VISIBILITY OF ADJACENT BUSINESSES, ESPECIALLY FREESTANDING SIGNS

MEYERS BIKE TRAIL

DESIGN PROGRAM:

- CLASS I BIKE TRAILS
- LANDSCAPE PLANTINGS, EROSION CONTROL
- STORMWATER RETENTION BASINS
- COMBINE DRIVEWAYS CROSSING TRAIL

LAKE VALLEY DISTRICT

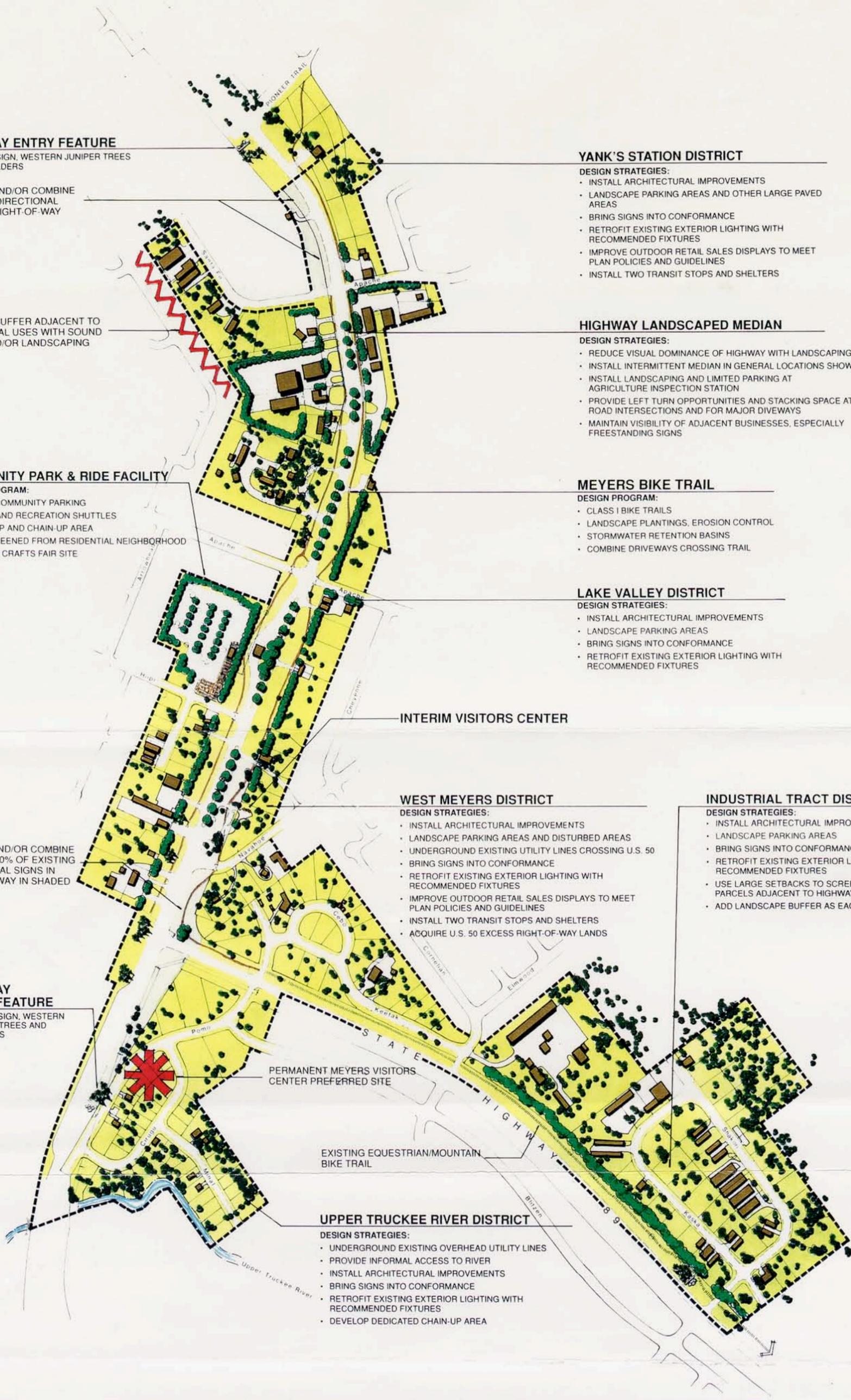
DESIGN STRATEGIES:

- INSTALL ARCHITECTURAL IMPROVEMENTS
- LANDSCAPE PARKING AREAS
- BRING SIGNS INTO CONFORMANCE
- RETROFIT EXISTING EXTERIOR LIGHTING WITH RECOMMENDED FIXTURES

INDUSTRIAL TRACT DISTRICT

DESIGN STRATEGIES:

- INSTALL ARCHITECTURAL IMPROVEMENTS
- LANDSCAPE PARKING AREAS
- BRING SIGNS INTO CONFORMANCE
- RETROFIT EXISTING EXTERIOR LIGHTING WITH RECOMMENDED FIXTURES
- USE LARGE SETBACKS TO SCREEN NEW DEVELOPMENT ON PARCELS ADJACENT TO HIGHWAY FROM SCENIC HIGHWAY
- ADD LANDSCAPE BUFFER AS EACH PARCEL IS DEVELOPED



Meyers Community Plan COMMUNITY DESIGN PLAN

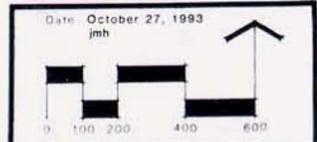
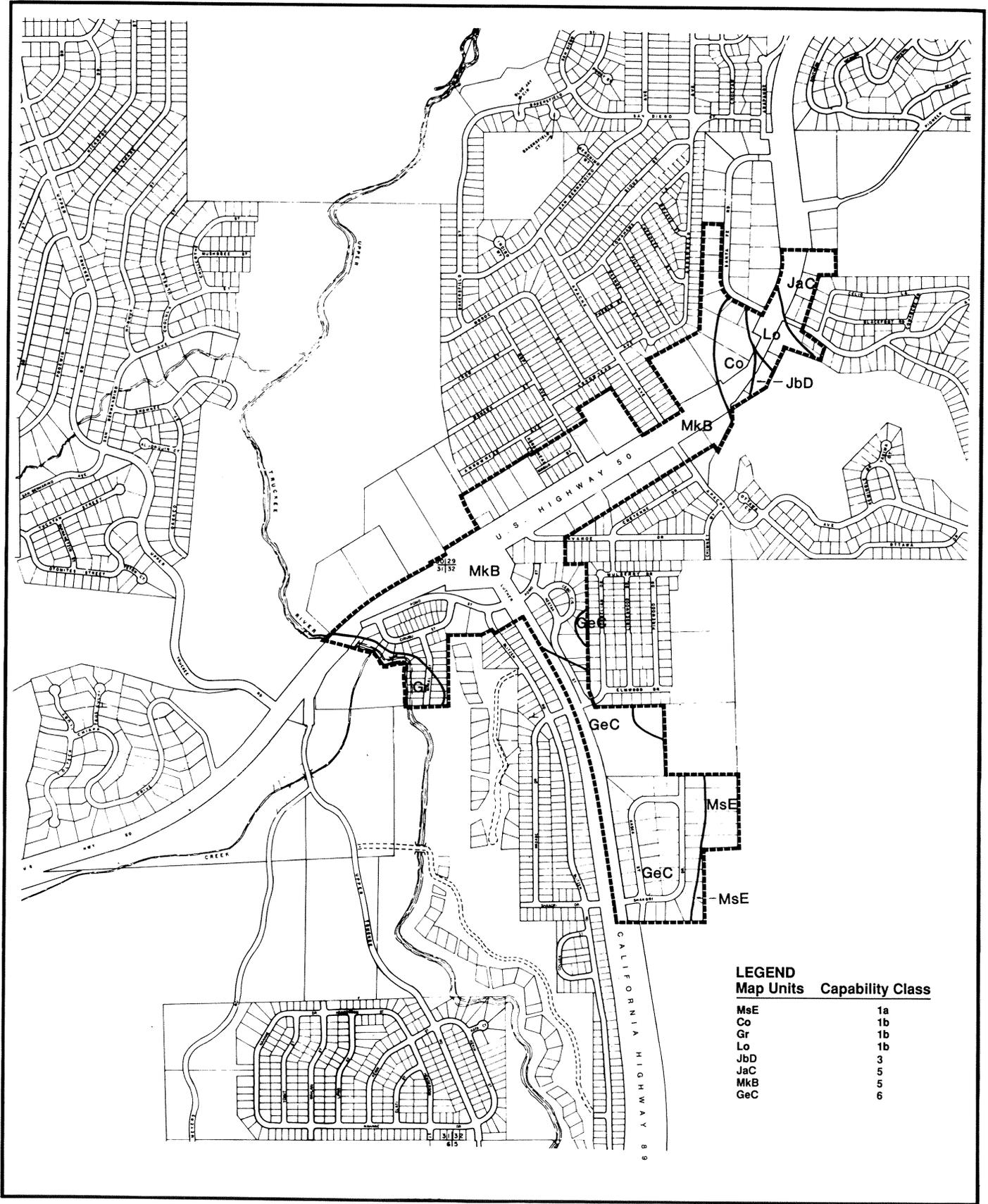


Figure 4-2. Community Design Plan



LEGEND

Map Units	Capability Class
MsE	1a
Co	1b
Gr	1b
Lo	1b
JbD	3
JaC	5
MkB	5
GeC	6



Meyers Community Plan

COMMUNITY PLAN LAND CAPABILITY

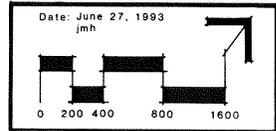


Figure 4-3. Community Plan Land Capability

C. LAND CAPABILITY MAPPING

A revised Bailey land capability map has been prepared by TRPA and is adopted as part of the community plan. See Figure 4-3. It replaces the existing 1987 TRPA land capability map for all lands within the community plan area boundary, and will become the official TRPA land capability overlay map. The revised map is essentially an area-wide land capability verification. The extent and location of existing soil map units were field verified. Soil map unit boundaries were revised where field data indicated differences from the adopted maps. No new soil map units were found. The revised mapping also includes revised stream environment zone (SEZ) delineations based on SEZ criteria established by the Water Quality Management Plan for the Lake Tahoe Region.

Generally, the 1987 map and the revised map are similar in terms of the relative acreage and areal distribution of each capability class. The revised map indicates approximately 9 acres less of class 1b (SEZ) lands than the 1987 adopted map (located in the vicinity of Yank's Station and the eastern edge of the Industrial Tract). It indicates a new area of class 3 near Yank's Station (previously mapped as class 1b), and additional area of class 6 near the Industrial Tract (previously mapped as class 5). Individual projects which are developed within the community plan area will have land capability verified as part of the project review process. The following table compares the approximate acreage of each capability class for both maps.

**Table 4-1
Comparison of 1987 and Community Plan Land Capability Maps**

Capability class	1987 Adopted Map (Acres)	1993 Community Plan Map (Acres)	Difference (Acres)
Class 1a	6.0	6.0	0.0
Class 1b	25.0	16.0	-9.0
Class 1c	1.0	0.0	-1.0
Class 3	0.0	3.0	+3.0
Class 5	101.0	92.0	-9.0
Class 7	22.0	38.0	+16.0
Totals	155.0	155.0	0.0

Source: Tahoe Regional Planning Agency, 1993.

Chapter 5

RECREATION ELEMENT

The recreation element is a supplement to the Recreation Element of the TRPA Goals and Policies Plan and the El Dorado County General Plan. It contains an inventory of the existing recreation opportunities in and around the plan area and identifies proposed improvements within the plan area.

A. EXISTING RECREATION OPPORTUNITIES

Meyers is surrounded by numerous recreation opportunities. The plan area contains the interim Meyers Visitors Center, the Pat Lowe Memorial Bike Trail (a Class I facility), a short stretch of the Upper Truckee River and an existing unpaved equestrian/bicycle trail along California 89. By virtue of its location, Meyers provides several recreation rental and sales businesses, including snowmobile tours and ski rentals. The eighteen hole Tahoe Paradise golf course is adjacent to the plan area and shares a parking lot within the community plan area with Yank's Station. In winter, the golf course is used for snowmobiling. Tahoe Paradise Park and Lake Baron are a short distance west of the plan area. National forest lands, Washoe Meadows State Park and California Tahoe Conservancy lands provide public open space near the Upper Truckee River. An existing unpaved trail leaves the end of Santa Fe Street and runs north along the former roadway alignment.

The following inventory identifies recreation providers presently located in or adjacent to the plan area:

1. USDA Forest Service Lake Tahoe Basin Management Unit (LTBMU). The LTBMU manages national forest lands within and surrounding the plan area (Tahoe Valley Management Area), providing open space and dispersed recreation opportunities. Within the plan area, the Meyers Work Center is a support facility for recreation and other national forest uses throughout the Tahoe Basin. In terms of TRPA's land use classifications, it is a public service use. The LTBMU is a partner in the existing, self-service Meyers Visitors Center. National forest lands adjacent to the plan area contain a restored wet meadow and an informal trail beginning at the end of Santa Fe Street along the former roadway alignment.
2. California Park Service (CPS). Formerly the California Department of Parks and Recreation, the CPS manages the nearby, undeveloped Washoe Meadows State Park. It is a partner in the Meyers Visitors Center.
3. California Tahoe Conservancy (CTC). The CTC manages several vacant parcels in and adjacent to the plan area, certain of which could potentially be used for public outdoor recreation or community facilities. It is a partner in the Meyers Visitors Center.
4. Tahoe Paradise Golf Course. Located adjacent to Yank's Station, it contains an eighteen hole golf course, driving range and clubhouse/ restaurant open to the general public. In winter, the golf course is used as a snowmobile course. Its parking area is on an adjacent parcel located in the community plan area. It is

privately owned.

5. El Dorado County Department of Parks and Recreation. It is responsible for providing recreation to county residents through the Tahoe Basin County Service Area. There are no existing facilities in or adjacent to the plan area.
6. Tahoe Paradise Resort Improvement District. The district owns and operates Tahoe Paradise Park, including Lake Baron. The park is a short distance west of the plan area. It is used by members of the Tahoe Paradise Resort Improvement District, an organization of Tahoe Paradise residential property owners.
7. Other Providers. Within the plan area, the Tahoe Paradise Winter Sports Center provides outfitter and guide services with snowmobile tours to locations outside the plan area. Of the three major ski areas located near Meyers, only Sierra-at-Tahoe (formerly Sierra Ski Ranch) provides skier and employee shuttle service to Meyers. Its primary stops are the Shell gas station and Meyers Downtown Cafe. Kirkwood and Heavenly Valley do not presently provide shuttle service to Meyers.

B. COMMUNITY PLAN RECREATION OBJECTIVES AND SPECIAL POLICIES

Utilizing the community plan goals established in Chapter 1, the following objectives and special policies shall be implemented as part of the plan. Objectives represent desired ends or results. Policies are implemented as the means to achieve objectives.

0 Recreation Goal

Expand recreational opportunities within the community plan area and provide access to nearby recreation.

Objectives and Special Policies:

1. Encourage the Forest Service and other partners to locate the interim and permanent interagency visitors center in Meyers.

Policy: The preferred site for the permanent Meyers Visitors Center is near the corner of U.S. 50 and California 89.

2. Encourage the Meyers Bike Trail to connect commercial areas with nearby recreation areas and trails.

Policy: Develop trail connections within and adjacent to the plan area. Specific projects include: constructing the second half of the Pat Lowe Memorial Bike Trail; a Class I trail linking Meyers with South Lake Tahoe; and trail connections between the plan area and nearby national forest, Tahoe Conservancy and Washoe Meadows State Park lands. Provide bike racks in land use districts to encourage bicycle use.

3. Encourage recreational uses in the community plan to which local businesses can provide support goods and services.

4. Discourage unscreened outdoor storage of recreational equipment which is visible from U.S. 50 or California 89.

Policy: Recreation projects shall be designed consistent with the Community Design Plan and Design Review Guidelines.

5. Provide parking and access to recreational opportunities in and around the plan area.

Policy: Improve transit opportunities that access recreation facilities on a year round basis. Examples include: extending summer beach bus service (S.T.A.G.E./Bus Plus) and winter ski shuttles to the plan area (Heavenly, Kirkwood and Sierra-at-Tahoe). Coordinate transit service with the community park and ride facility.

C. COMMUNITY PLAN RECREATION IMPROVEMENTS

The plan envisions the following recreation improvements to implement the goal and objectives.

1. USDA Forest Service Lake Tahoe Basin Management Unit. The LTBMU intends to develop the permanent Meyers Visitors Center to replace the existing interim facility. Its estimated capacity is 150 persons at one time (PAOTs). The project is on TRPA's Five Year List of Additional Recreation Facilities. It requires an allocation from TRPA of Summer Day Use PAOTs as part of the project review and approval process. No specific projects are proposed by LTBMU's Land and Resource Management Plan for areas surrounding the plan area.

LTBMU's general management prescriptions include continuing developed recreation at the existing recreation residence tracts along the Upper Truckee River and timber stand maintenance along the eastern edge of the Industrial Tract. The existing Work Center is expected to remain in its present capacity.

2. California Park Service. The Park Service is expected to continue as a partner in developing the permanent Meyers Visitors Center. They intend to prepare a general plan for the Washoe Meadows State Park. The plan will recommend developing a trail connection from their boundary near U.S. 50 and the Upper Truckee River into the park. It may investigate the potential to add an undercrossing of U.S. 50 to reach the Visitors Center.
3. California Tahoe Conservancy. The Conservancy is expected to continue as a partner in developing the permanent Meyers Visitors Center. Conservancy land within the plan area is a potential site for the community park and ride facility. It may also provide land acquisition assistance if the facility is developed on the vacant parcel owned by the State's Department of Agriculture adjacent to the existing Meyers Agricultural Inspection Station.
4. Tahoe Paradise Golf Course. The Tahoe Paradise golf course and winter snowmobile course is expected to remain in its present form. The opportunity exists to provide a small outdoor retail/cafe area at the edge of the golf course at the rear of Yank's Station. A portion of the project could be developed as an accessory use to the golf course and not require a commercial floor area allocation. The Yank's Station complex could be redeveloped to provide access to the rear of the site. Regardless of the

manner in which it is developed, the edge between the uses and plan areas could become an asset to the area. An SEZ restoration project containing an existing channel which drains part of the golf course is recommended by the plan. The channel is on the boundary between Yank's Station and the golf course. See Chapters 4 and 7.

5. El Dorado County Service Area. No improvements identified at this time within the plan area.
6. Tahoe Paradise Resort Improvement District. No improvements identified at this time within the plan area.
7. Other Providers. The plan recommends expanding winter ski shuttle and summer beach bus service to the community park and ride facility. Heavenly Valley, Kirkwood and Sierra-at-Tahoe will be requested to provide regular shuttle service. S.T.A.G.E. will be requested to provide connecting summer beach bus service. Bicycle racks or lockers should be provided at the park and ride facility.

Chapter 6

PUBLIC SERVICE ELEMENT

The public service element is a supplement to the Public Service Element of the TRPA Goals and Policies Plan and the El Dorado County General Plan. It contains an inventory of the existing public services within the plan area. Further, it establishes Plan policies addressing public service and identifies anticipated public service improvements within the plan area.

A. EXISTING PUBLIC SERVICE FACILITIES

As the primary commercial and light industrial area in the El Dorado County portion of the Tahoe Basin, Meyers contains nearly all the offices, facilities and maintenance yards of local and sub-regional public service providers. It includes the Meyers post office, state and local government services as well as regulated utilities. The plan envisions Meyers to continue to be the primary location for public services in the county. Public service improvements contemplated by the plan are based on proposed projects contained on TRPA's Five Year List of Additional Public Service Facilities or on discussions with public service providers. Public transportation policies and proposed improvements are found in Chapter 3. The following inventory identifies public service providers presently located in the plan area. See Figure 6-1.

U.S. Postal Service. The Postal Service presently operates the Tahoe Paradise Station in the Meyers Downtown Cafe building on U.S. 50. It contains a full service counter, general delivery service and approximately 768 individual post boxes.

California Highway Patrol (CHP). The CHP's Area Command Facility is located on Navahoe Drive along the eastern edge of the plan area. The facility houses approximately fifteen officers and staff. It is responsible for patrols in the southern end of the Tahoe Basin and from Twin Bridges on U.S. 50 to the Amador County line on California 88. The facility, which is owned by the CHP, has been in operation for over twenty five years in Meyers.

California Department of Transportation. The Caltrans Meyers Maintenance Station is located along California 89 near the Cornelian Drive intersection. The station is approximately ten acres in size and contains vehicle storage, maintenance and repair garages, and dormitory and cafeteria facilities for winter snow removal crews. It provides road maintenance and construction, snow removal and communications services to nearby areas within and outside the basin.

California Department of Food and Agriculture, Division of Pest Exclusion. The Department of Food and Agriculture maintains the Meyers Inspection Station along U.S. 50 near the center of the plan area. The inspection station's role is to prevent unwanted pests, hosts or other organisms from entering the state which may threaten the state's agricultural crops.

California Conservation Corps (CCC). The CCC is presently housed in an existing thirty seven unit motel behind Yank's Station on Apache Avenue. The site is used as a residential dormitory for up to eighty five members and includes food service and administrative offices in the nearby Yank's Station complex. The Corps' mission is to develop California's youth between the ages of 18 and 23 and enhance the state's natural resources. It engages in erosion control and timber stand maintenance projects on a year round basis.

Lake Valley Fire Protection District. The district maintains the Lake Valley fire station and training center on a one and one-half acre site located on Keetak Street near California 89. The site houses a three bay fire station, living quarters, a practice burn tower and district administrative offices.

El Dorado County Department of Transportation (DOT). El Dorado County's road maintenance and snow removal facility is located on a two acre site on Shakori Street in the Industrial Tract. The facility also houses erosion control engineering, vehicle storage and repair and storage of construction materials. DOT recently developed the first phase of the Pat Lowe Memorial Bike Trail along the south side of U.S. 50. The trail is a Class I facility meaning it is physically separated from the nearby highway, and has a certain minimum width.

El Dorado County Department of Animal Control. El Dorado County's animal control facility and shelter is located on Shakori Street in the Industrial Tract. The facility provides animal control, pet adoption and shelter services. It formerly shared space there with the South Lake Tahoe Humane Society which now is located in the West Meyers area.

South Lake Tahoe Humane Society. The society operates out of a recently acquired site on Magua Street in West Meyers.

South Tahoe Public Utility District (STPUD). STPUD's main sewage export transmission pipeline serving the south shore runs along U.S. 50 and California 89 through the plan area. STPUD provides water and sewer service to developed uses in the plan area. In 1989, the District purchased the Tahoe Paradise Water Company including two parcels of land and two older buildings on Santa Fe Street. One parcel contains the two structures one of which is leased to Simmons Cable Television, while the second serves as a lunchroom and materials storage. The second parcel is vacant.

Pacific Bell Telephone. Pacific Bell operates the Meyers Central Office, an unmanned switching station at a site near the intersection of U.S. 50 and Apache Avenue.

Simmons Cable Television. Business offices and receiving/ transmitting facilities for the Simmons cable television system are located on Santa Fe Street at a site presently owned by the South Tahoe Public Utility District.

USDA Forest Service Lake Tahoe Basin Management Unit (LTBMU). The LTBMU operates the existing Meyers Work Center located along U.S. 50 in the Lake Valley land use district. It provides several services and facilities to the LTBMU throughout the Region. It contains dormitory space to house temporary or detail staff, a maintenance shop and laboratory.

Other Public Service Providers. Natural gas service is provided in the plan area by Washington Power Natural Gas (WPNG). Electrical service is provided by Sierra Pacific Power Company (SPP). In 1986 and 87, over head electrical lines located along U.S. 50 through portions of the plan area were converted to underground facilities. Telephone and cable television lines which were on SPP's overhead poles were also converted to underground service at the same time. Existing overhead utility lines run along U.S. 50 west of the California 89 intersection and along Keetak Street near the Industrial Tract.

B. COMMUNITY PLAN PUBLIC SERVICE OBJECTIVES AND SPECIAL POLICIES

Utilizing the community plan goals established in Chapter 1, the following objectives and special policies shall be implemented as part of the plan. Objectives represent desired ends or results. Policies are implemented as the means to achieve objectives.

θ Public Service Goal

Support development of public service projects within the plan area which support the community.

Objectives and Special Policies:

1. Ensure all public service projects implement the Community Design program.

Policy: Public service projects shall be designed consistent with the Community Design Plan and Design Review Guidelines.

Policy: The California Department of Transportation is requested to remove and consolidate existing highway directional signs within the plan area wherever feasible.

Policy: Sierra Pacific Power Company and other utility providers with overhead utility lines along designated scenic highway corridors are requested to convert the lines to underground facilities within the plan area.

2. Incorporate the post office as an anchor in one of the commercial land use districts.

Policy: The U.S. Postal Service is requested to develop its new facility as one which does not require the allocation of additional commercial floor area in either the Yank's Station or West Meyers land use cluster. It is further requested use the name of Meyers for the new facility.

3. Encourage the Department of Food and Agriculture to consider, in its long range facilities planning, to relocate the existing inspection station outside of the community plan area. Until such relocation, encourage it to retrofit the existing station consistent with the Community Plan Design Review Guidelines.

Policy: [§]The Department of Food and Agriculture is requested to implement Community Design improvements at its existing inspection station. The Department is further requested to work with the California Tahoe Conservancy, El Dorado County, TRPA, and transit service providers to develop the site adjacent to the existing inspection station into a community

[§] Amended 12/20/95

facility including park and ride and community events uses.

4. Encourage the California Highway Patrol to remain within the community plan area.

Policy: The plan supports development of new or expanded California Highway Patrol offices in the plan area.

5. §Identify a building or site for a Meyers Town Hall for town meetings and other community functions.

Policy: The South Tahoe Public Utility District is requested to consider providing shared public meeting space at its existing Santa Fe Street site.

6. Encourage new and expanding public service uses to be developed in such a manner as to not need additional commercial floor area which may be allocated through the community plan.

Policy: New and expanding public service uses shall not utilize additional commercial floor area allocated by the community plan.

7. Support development of a preschool/day care facility in or adjacent to the community plan in a location which is safe and would not conflict with residential uses.

8. Encourage the California Conservation Corps to retrofit its existing facilities consistent with the Community Plan Design Review Guidelines.

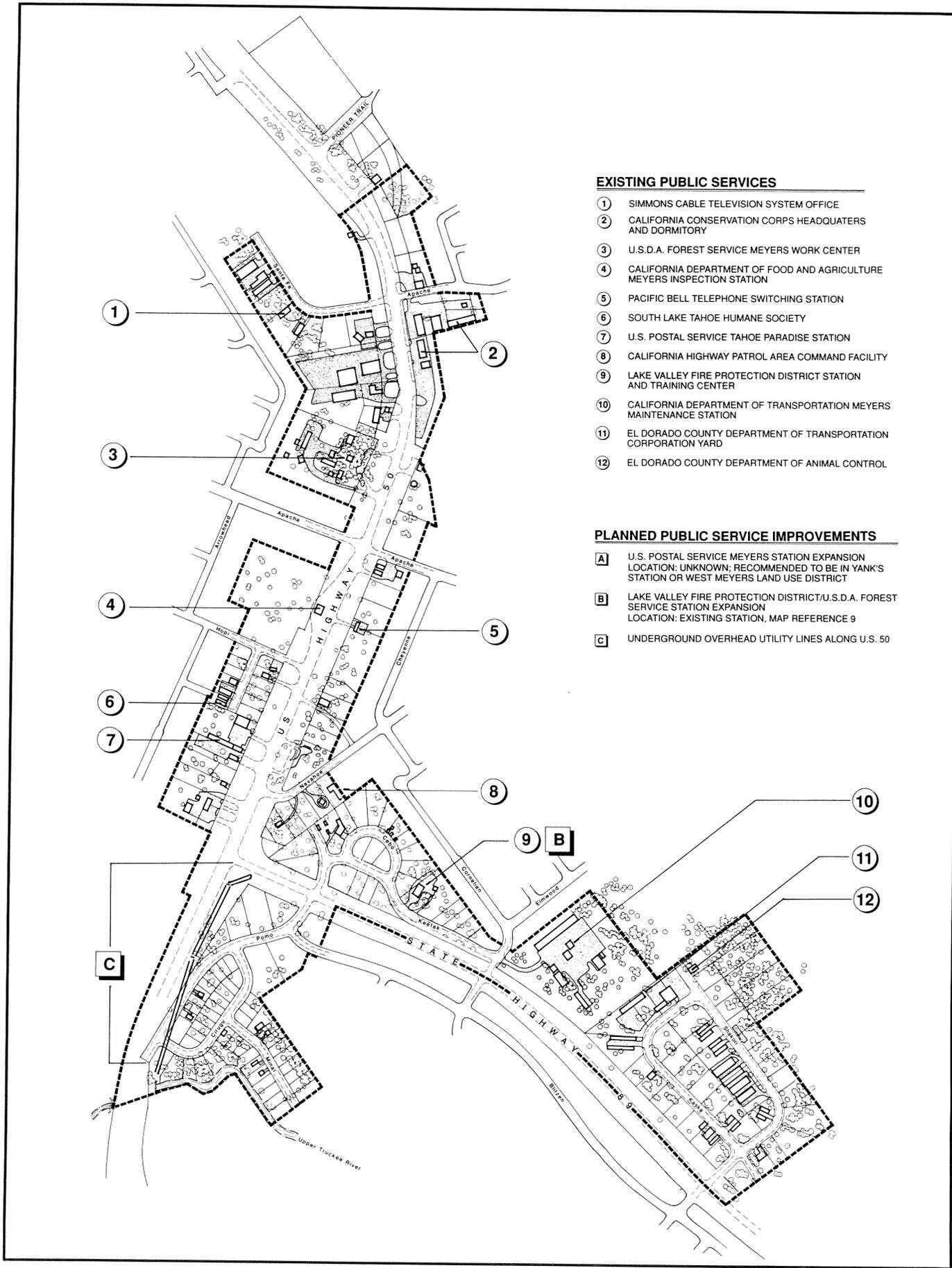
9. Encourage new and relocated public service corporation yards and storage facilities to locate in the Industrial Tract land use district.

C. COMMUNITY PLAN PUBLIC SERVICE IMPROVEMENTS

The plan envisions the following public service improvements to implement the goal and objectives and to maintain or improve service capacities. Several of the identified improvements appear on TRPA's Five Year List of Additional Public Services. See Figure 6-1.

1. U.S. Postal Service. The Postal Service is planning to occupy a new facility within the community plan area. The proposed facility is expected to be larger and offer more services, including home mail delivery, than the existing station located in West Meyers.
2. California Highway Patrol. The CHP has identified the need for larger offices. It is considering moving out of the plan area to accommodate its needs. It may be possible to expand the existing site to meet space needs. If they move, the existing site may be a potential public meeting space.
3. California Department of Food and Agriculture, Division of Pest Exclusion. The Department of Food and Agriculture has no immediate plans to change the existing Meyers Inspection Station. Its long range objectives include the possibility of

§ Amended 12/20/95



EXISTING PUBLIC SERVICES

- ① SIMMONS CABLE TELEVISION SYSTEM OFFICE
- ② CALIFORNIA CONSERVATION CORPS HEADQUARTERS AND DORMITORY
- ③ U.S.D.A. FOREST SERVICE MEYERS WORK CENTER
- ④ CALIFORNIA DEPARTMENT OF FOOD AND AGRICULTURE MEYERS INSPECTION STATION
- ⑤ PACIFIC BELL TELEPHONE SWITCHING STATION
- ⑥ SOUTH LAKE TAHOE HUMANE SOCIETY
- ⑦ U.S. POSTAL SERVICE TAHOE PARADISE STATION
- ⑧ CALIFORNIA HIGHWAY PATROL AREA COMMAND FACILITY
- ⑨ LAKE VALLEY FIRE PROTECTION DISTRICT STATION AND TRAINING CENTER
- ⑩ CALIFORNIA DEPARTMENT OF TRANSPORTATION MEYERS MAINTENANCE STATION
- ⑪ EL DORADO COUNTY DEPARTMENT OF TRANSPORTATION CORPORATION YARD
- ⑫ EL DORADO COUNTY DEPARTMENT OF ANIMAL CONTROL

PLANNED PUBLIC SERVICE IMPROVEMENTS

- A U.S. POSTAL SERVICE MEYERS STATION EXPANSION
LOCATION: UNKNOWN; RECOMMENDED TO BE IN YANK'S STATION OR WEST MEYERS LAND USE DISTRICT
- B LAKE VALLEY FIRE PROTECTION DISTRICT/U.S.D.A. FOREST SERVICE STATION EXPANSION
LOCATION: EXISTING STATION, MAP REFERENCE 9
- C UNDERGROUND OVERHEAD UTILITY LINES ALONG U.S. 50



**Meyers Community Plan
PUBLIC SERVICES**

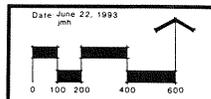


Figure 6-1. Public Services

relocating the inspection station to a point further west past the intersection of U.S. 50 and California 89. The community plan envisions the state-owned parcels adjacent to the station to be developed with the department's assistance to a community park and ride and temporary events site.

4. California Conservation Corps. The Corps plans to maintain its presence in the Tahoe Basin and may seek to develop a more permanent facility in the future outside of Meyers. This may have secondary or spin-off benefits to Meyers by returning the site to its original motel use.
5. Lake Valley Fire Protection District. The district proposes to expand the existing station cooperatively with the USDA Forest Service. Their proposal includes additional vehicle bays and barracks. Additionally, it proposes to add a new storage building for firefighting and snow removal equipment and construct a physical agility course on the existing site.
6. El Dorado County Department of Transportation. DOT has recently completed the addition of a meeting room and additional office space. The second phase of the Pat Lowe Memorial Bike Trail is planned for construction in 1994.
7. Place Existing Overhead Utility Lines Underground. Sierra Pacific Power Company and other utility providers using overhead facilities which are located along U.S. 50 (a state and regional designated scenic highway corridor) are requested to convert those systems to underground facilities within the plan area. Statewide undergrounding conversion funds are available to make the conversion. See Chapter 4 for additional discussion.
8. USDA Forest Service Lake Tahoe Basin Management Unit (LTBMU). The LTBMU has identified no specific changes to the Work Center, although the site may be considered as a potential site for a relocated supervisor's office.

No other planned public service improvements are known at this time.

Chapter 7

IMPLEMENTATION ELEMENT

While the plan sets forth the framework for achieving the desired goals and objectives, it is the day to day actions of government which truly shape the community plan area. Thus, the manner in which the government and the community implement the plan is the real test of commitment to its goals and objectives. The implementation element is a supplement to the Implementation Element of the TRPA Goals and Policies Plan and the El Dorado County General Plan.

The Meyers Community Plan must respond to and be con with the goals, objectives and requirements of both El Dorado County and TRPA. The requirement to prepare and implement a Meyers Community Plan arises from the Regional Plan Goals and Policies and is authorized by the California State Government Code. The plan, in the context of California state planning law, serves as the General Plan for the geographic area defined by the Meyers Community Plan.

The plan is adopted by the County Board of Supervisors as an amendment to the El Dorado County General Plan. It is adopted by the TRPA Governing Board as a Regional Plan amendment which replaces the existing Plan Area Statement for Plan Area 125, Meyers Commercial. As a result of plan adoption, projects proposed within the plan area must be consistent with not only the community plan itself, but also with the El Dorado county General Plan and TRPA's Regional Plan.

This chapter sets forth the mechanisms and actions necessary to carry out the goals and objectives of the Community Plan.

A. COMMUNITY PLAN IMPLEMENTATION OBJECTIVES AND SPECIAL POLICIES

Utilizing the community plan goals established in Chapter 1, the following objectives and special policies shall be implemented as part of the plan. Objectives represent desired ends or results. Policies are implemented as the means to achieve objectives.

θ Implementation Goal

Develop realistic implementation methods to ensure the community plan's long term success.

Objectives and Special Policies:

1. The El Dorado County Board of Supervisors or their designee shall have the responsibility to guide the plan's implementation[§]

Policy: Land coverage, commercial floor area, tourist accommodation units, and other units of use regulated by TRPA shall not be transferred out of the plan area unless the Board of Supervisors or their designee finds that such transfer will result in a substantial community benefit in terms of implementing the community plan and its programs[§].

[§] Amended 12/20/95

2. Identify appropriate entities to take responsibility for implementing and monitoring the plan's improvement programs.

Policy: Projects utilizing the Bonus Unit Incentive Program provided under Chapter 35 of TRPA's Code of Ordinances to acquire bonus tourist accommodation units shall participate in capital improvement projects within the community plan under Section 35.3.

Policy: Off-site mitigation which may be required as part of a man-modified land capability determination shall be implemented within the community plan area.

Policy: Projects containing existing land coverage greater than 70% shall reduce on-site coverage by a minimum of 5% as part of the project.

Policy: Project applicants who elect to participate in substitute air quality or water quality mitigation fee program(s) shall implement such programs or projects within the community plan area.

3. Establish community design criteria to evaluate distribution of additional commercial floor area.

Policy: Projects seeking allocations of additional commercial floor area within the plan area shall be subject to the plan's Commercial Floor Area Allocation Procedures.

4. Request, as part of plan adoption, that the Tahoe Regional Planning Agency allow use of additional floor area allocation for projects within the community plan which had been reserved for projects outside the community plan.

Policy: In the event that additional commercial floor area is allocated to the plan area, it shall be distributed to each priority group identified in Appendix C in the same proportions as the existing allocation unless the El Dorado County Board of Supervisors finds modification is needed to meet current trends, and such modified allocation maintains the intent of the Meyers Community Plan. [§]

5. Identify program cost estimates and potential funding sources for improvement programs contained in the final community plan. Hold community fund raising events to raise money and develop awareness of the plan.
6. Consider the feasibility of establish of establish local funding sources such as improvement or assessment districts as a means to implement the plan.
7. Identify and encourage opportunities to relocate and consolidate existing commercial uses within the community plan.
8. Incorporate the plan into the County's 2010 General Plan and initiate rezoning of the lands within the community plan as necessary to implement the plan and establish consistency between them.
9. Request that El Dorado County make available previously banked land

[§] Amended 05/27/98.

- coverage for use as an incentive in the community plan area.
10. Request Caltrans to make available for sale or other means of transfer the excess U.S. 50 right of way in West Meyers to the adjoining property owners.

B. IMPLEMENTING THE COMMUNITY PLAN

The plan will implement the following items. References are provided to locate additional discussion of each item.

1. Allocate Additional Commercial Floor Area and Tourist Accommodation Units. The plan allocates 25,400 square feet of additional commercial floor area (CFA) and ten additional bonus tourist accommodation units (TAU) to new and existing uses within the plan area. The CFA will be allocated according to allocation guidelines established in Appendix C of the plan. Uses must meet the guidelines and other applicable portions of the Regional Plan to be allocated the CFA. The CFA is part of an incentive package offered to stimulate development or private and public improvement projects contained in the plan. [§]

The additional ten bonus TAUs contained in the plan do not have specific plan guidelines or regulations regarding their allocation. The Regional Plan requires projects using bonus TAUs to match each additional unit with an existing transferred unit. Projects must also participate in identified capital improvements established by Chapter 35 of TRPA's Code to accumulate bonus units.

2. Implement Capital Improvement Programs. A series of capital improvement programs will implement the plan's goals, objectives and policies. They will assist in achieving environmental threshold-related targets identified in Chapter 4. The programs are fully described in this chapter.

Following plan adoption, the Board of Supervisors or their designee will develop and adopt, with the assistance of TRPA, a prioritized list of public improvements. Preliminary program costs, funding sources, and other mechanisms shall be identified to carry out the projects over a specified schedule^{§§}.

To fully realize the plan's vision, funding for numerous improvements will be needed. The plan's philosophy regarding funding advocates two complimentary approaches: new construction and remodel improvements on private lands would be funded by the individual property owners or tenants who directly benefit from the improvements; public improvements constructed on public lands would be funded through a combination of capital improvements funds and grants. For the purpose of the plan's capital improvement programs, El Dorado County is identified as the recipient of grants and mitigation fees.

The plan recommends establishing a low interest, revolving building improvement loan program with area banks to help provide financial resources to carry out improvements on private lands. Interested community leaders should cooperatively approach area banks to establish the program. Seed money to begin the program may come from the following source: area banks and other lending institutions; State of California Main Streets Program grant; shared participation among businesses within the plan area; and creation of a County Redevelopment district^{§§}.

[§] Amended 05/27/98.

^{§§} Amended 12/20/95

Public improvements will seek state and federal funds for highway-related projects. State grants and regional mitigation fees will be sought to construct the balance of public projects which can be shown to implement the programs' objectives. Other publicly oriented projects such as the Meyers Visitors Center will be funded through programs of the individual agencies involved. Based on the perceived or actual success or failure of funding and implementing plan improvements, El Dorado County may pursue creation of a redevelopment district and agency provided by California state law. It could be created during any part of the plan's life, however, in general the earlier it is implemented, the greater the chance for its success.

3. Improve the Visual and Scenic Quality of Meyers. Central to the plan's implementation is the Community Design Plan, Design Review Guidelines and substitute sign standards. Their purpose is to improve the appearance and function of the built environment in Meyers and create a visually memorable gateway to Lake Tahoe. U.S. 50 and California 89 are state designated scenic highways and TRPA-designated regional scenic corridors. Physical improvements to the built environment are needed to meet perceptual expectations of persons travelling the scenic corridors. Both elements will be a positive catalyst for future development and redevelopment in Meyers. They apply to all projects in the plan area regardless of ownership. Specific program policies are listed in Chapter 2. The Community Design Plan is located in Chapter 4. Design Review Guidelines are located in Appendix A. Substitute sign standards are found in Appendix B. They will replace certain sections of existing County and TRPA standards and guidelines within the plan area.

Community design elements are implemented as part of the project review and approval process. Prior to project approval, a finding of consistency shall be made with the design program goals, objectives, and specific design criteria[§].

Substitute sign standards recognize the unique environment in Meyers and will replace specific sections of existing TRPA and County standards. Certain sections of the Region-wide ordinance will continue to be in effect (e.g., amortization of non-conforming signs), however, the determination of non-conformity will be based on the substitute standards wherever they apply.

4. Eliminate Inconsistencies Between El Dorado County Zoning and TRPA's List of Permissible Uses in the Plan Area. Several inconsistencies exist between the two sets of regulations pertaining to allowable uses and their definitions within the plan area. Adoption of the plan and its implementing ordinances will achieve consistency between the El Dorado County zoning ordinance and TRPA's list of permissible land uses.

TRPA will adopt the Community Plan Area Statement (Chapter 2) as part of adopting the plan and replace the existing Plan Area Statement 125, Meyers Commercial. El Dorado County will incorporate the community plan as adopted verbatim and in total into the 2010 General Plan when it is adopted. In the interim, it will amend the County's existing 1969 General Plan and 1981 Long Range Land Use Plan.

[§] Amended 12/20/95

Use provisions and development standards of the County zoning ordinance will be amended to be consistent with the Community Plan and associated Plan Area Statement.

C. METHODS OF IMPLEMENTATION

The following methods will be used to implement the plan. Additional methods may prove to be effective in implementing the plan.

1. Provide Additional Development Incentives. The plan contains at least three significant incentives to create additional growth and make badly needed improvements to the area. The incentives are: 25,400 square feet of additional commercial floor area; ten additional tourist accommodation bonus units; and additional land coverage.[§]

Additional commercial floor area is acquired through the allocation procedures listed in Appendix C. Tourist accommodation bonus units are acquired through participation in the Bonus Unit Program prescribed in Chapter 35 of TRPA's Code. Additional land coverage which can range from 50% on developed parcels to 70% on undeveloped parcels is permitted in certain situations by Chapter 20 of the Code.

2. Planning Commission Designated. The Board of Supervisors designates the Planning Commission as the primary implementing body of the community plan. It has specific duties in several areas including design review, determining consistency with plan policy, and identifying the need for plan amendments^{§§}.
3. Acquire and Utilize the Excess U.S. 50 Right-of-Way. Essential to the plan's implementation is the integration of the two forty foot strips of U.S. 50's non-operational right-of-way. The right-of-way is controlled by Caltrans. The majority of the excess right-of-way is located in the West Meyers land use district. Through sale or other means of transfer to adjoining property owners, the unusually wide swath of no man's land is expected to gradually disappear. Minor boundary lot line adjustments should be used to revise parcel boundaries to include the former right-of-way. Chapter 2 identifies the appropriate uses permissible within these lands.
4. Use Substitute Mitigation Fee Program. The Regional Plan allows projects within community plans to participate in substitute mitigation fee programs in the areas of excess land coverage, water and air quality. Substitute fee programs generally allow project proponents to mitigate impacts off-site in lieu of paying mitigation fees. The benefit of substitute programs is that mitigation work can be performed within the plan area. Smaller, individual projects which tend to have smaller mitigation fees are generally not expected to participate in the substitute programs.

The plan recommends participation by larger projects in substitute air and water quality mitigation programs. Due to the complexity of the program, the plan does not generally recommend development of a community plan excess land coverage reduction program.

- a) Water Quality Mitigation Fee Program. Water quality mitigation fees are collected from projects which add impervious surfaces such as

[§] Amended 05/27/98.

^{§§} Amended 12/20/95

buildings and pavement. The fees are used to fund water quality improvement projects in the jurisdiction in which they are collected. Fees are collected for each additional square foot of coverage added. Chapter 82 of the Code allows individual projects to perform off-site water quality mitigation as a part of the project of paying the mitigation fee. Consistent with the policy established above, projects which elect to use the substitute fee program shall participate in a program or project located within the community plan area.

The plan contains a water quality improvement program consisting of specific water quality improvement projects which should be constructed within the plan area. Refer to Chapter 4 and this chapter for a detailed program description. Individual public and private development projects which install all or parts of the identified off-site improvements as part of their project may be exempted from the water quality mitigation fees in an amount equal to their contribution.

- b) Air Quality Mitigation Fee Program. Air quality mitigation fees are collected for projects which either produce or attract additional vehicle trips. The fees are used to fund transportation and air quality improvement projects in the jurisdiction in which they are collected. Generally, commercial and public service uses attract trips (e.g., a new retail store), while tourist accommodation and residential uses (beds) produce trips. Fees collected are based on either the number of new trips produced or the number attracted. Chapter 93 of the Code allows individual projects to perform off-site transportation and air quality mitigation as a part of the project instead of paying the mitigation fee. Consistent with the policy established above, projects which elect to use the substitute fee program shall participate in a program or project located within the community plan area.

Chapter 4 of the plan contains a transit/transportation/air quality improvement program consisting of specific improvement projects which should be constructed or implemented within the plan area. Refer to Chapter 4 and this chapter for detailed program descriptions. Individual public and private development projects which install all or part of the identified off-site improvements as part of their project may be exempted from the air quality mitigation fees in an amount equal to their contribution.

5. Implement Community Design Improvements Program Using Conditions of Project Approval. An important method to realize certain improvements or implement specific plan policies is to require them to be installed or implemented as a condition of project approval. The improvements, which generally are needed on-site, are then implemented at the same time as the rest of the project. On-site improvements which the plan recommends to be implemented as a condition of individual project approval generally include landscaping, building façade changes, signage, parking and access, exterior lighting, bicycle racks and outdoor displays. The Community Design Plan indicates the general location and nature of planned improvements. It serves as the long range vision for the plan area. All projects should be consistent with the map. §The Planning Commission of the Planning

§ Amended 12/20/95

Department as may be applicable, should review projects and find them to be consistent with applicable portions of the plan prior to recommending their approval, or taking final action when appropriate.

6. Adopt a Unified Set of Permissible Land Uses . A major constraint to orderly land use and development in Meyers is the existing County zoning. In several instances, it does not permit certain land use otherwise allowed by TRPA. To receive approval, a project must be a permissible use by both the County and TRPA. An important plan objective, therefore, is to remove the inconsistency between the two jurisdictions. As part of adopting the community plan and implementing their General Plan, El Dorado County will revise the County zoning to be consistent with permissible uses listed in the Community Plan Area Statement (Chapter 2).

D. SCHEDULE OF PLAN IMPLEMENTATION

Implementing the plan will take several years. It is unreasonable to assume that all improvements and all additional development included in the plan will occur immediately following the plan's adoption. Plan implementation can be broken down into three types of activities: development of additional commercial floor area and tourist accommodation units; rehabilitation of private land uses; and installation of public benefits and improvements. Implementation of the plan should begin with enforcement of existing County and TRPA land use and development ordinances. Lack of consistent enforcement of existing codes and ordinances, especially along the U.S. 50 corridor, is a contributing cause to significant negative visual quality impacts.

While the initial plan contains commercial floor area allocations only through 1997, it should be viewed as an on-going plan for development of Meyers. The plan is reviewed and adjusted every five years. It advocates the following implementation schedule using five year increments.

θ Years 1-5

- Commercial floor area allocation distributed and constructed (includes installation of community design, signage and water quality improvements on parcels receiving allocation)
- Pat Lowe Memorial Bike Trail completed (includes removal and restoration of excess land coverage and disturbed lands, and restoration of associated SEZ areas)
- Transfer and acquisition of U.S. 50 excess right-of-way
- Non-conforming signs replaced and/or removed
- Excess highway signage consolidated and/or removed
- Park and Ride facility developed
- Community gateway entry features constructed
- Overhead utility lines converted to underground service along U.S. 50
- Water quality Best Management Practices installed on all parcels

θ **Years 6-10**

- Additional tourist accommodation units distributed and constructed
- Remaining SEZ areas restored
- Meyers Visitors Center developed (permanent facilities)
- Agricultural Inspection Station architectural improvements installed
- Architectural improvements completed on all uses with U.S. 50 frontage
- Designated chain-up area constructed

θ **Years 11-15**

- Landscaping improvements completed on all uses with U.S. 50 frontage
- Recreational access trailheads developed
- U.S. 50/California 89 intersection improvements installed
- Water quality Best Management Practices installed on public rights-of-way

θ **Years 16-20**

- U.S. Highway 50 landscape medians and turn pockets installed

E. CAPITAL IMPROVEMENT PROGRAMS[§]

The following list of capital improvement program (CIP) projects is expected to be implemented within the plan area to achieve the plan's goals, objectives and improvement targets.

Project schedules and cost estimates are preliminary and are subject to change. Where shown, project costs are estimated as mid-range costs.

WATER QUALITY

1. Stream Environment Zone (SEZ) Restoration

Purpose: To implement the TRPA SEZ Restoration Program for the Meyers area and to achieve the SEZ restoration target established in Chapter 4.

Program Description: To achieve the community plan SEZ restoration target of 1.5 acres, the following projects will be implemented:

- a) Minal Street cul-de-sac (0.25 acres)
- b) U.S. 50 Yank's Station area sliver parcels (3 parcels/1.1 acres)
- c) Yank's Station drainage channel (0.15 acres)

Estimated Cost:

a) Minal Street cul-de-sac	\$100,000
b) U.S. 50 Yank's Station area sliver parcels	\$75,000

[§] Amended 12/20/95

Note: A portion of the costs are included in bike trail CIP below

c) Yank's Station drainage channel \$40,000

Funding: California Tahoe Conservancy Restoration grant; Caltrans Transportation Enhancement Activities grant; TRPA water quality mitigation funds; Private

AIR QUALITY/TRANSPORTATION

1. Pat Lowe Memorial Bike Trail, Phase 2

Purpose: To promote pedestrian and bicycle use, thereby reducing automobile dependency for short trips within the plan area.

Program Description: Construct a Class I bike trail along the northside of U.S. 50. The project will include landscaping, restoration of stream environment zone and other disturbed lands and water quality control facilities.

Estimated Cost: \$450,000

Funding: California Tahoe Conservancy Recreation Access grant; Caltrans Transportation Enhancement Activities grant; TRPA water/air quality mitigation fund; County Transportation Development Act (TDA) fund, Burton-Santini funds; State Water Quality Control Board Non-point Source Pollution Control grant.

2. El Dorado County Transit Service Expansion

Purpose: To meet targets for vehicle miles travelled and maintain level of service at key intersections.

Program Description: Increase headways to one hour and hours of operation to 6:30 a.m. to 11:30 p.m. for Bus Plus (STAGE) service to the plan area; add additional transit stops and five shelters at key locations.

Estimated Cost:

5 Shelters @ \$12,000 per shelter \$60,000
Expanded Service* \$2,160,000

Funding: El Dorado County State Transit Assistance funds; TRPA air quality mitigation funds; Private; Federal Transit Administration Section 3 and Section 18 grants.

* Cost shown is for total El Dorado County transit service expansion during the first five years of Regional Transportation Plan implementation. Increased service to Meyers is included in the cost.

3. Community Park and Ride Facility

Purpose: To provide an integrated community transit and parking facility reducing vehicle trips and increasing access to transit service. It may also serve as a rest stop and winter chain-up area for motorists travelling through

Meyers.

Program Description: Develop two state-owned, 4.59 acre parcels along U.S. 50 into an integrated transit center with community parking for approximately 150 vehicles, transit and other recreation shuttle service (ski areas), restrooms, bicycle racks or locker, storm water drainage, signage and information kiosk. A long term goal is to have south shore employee van/bus shuttle originate from the facility. Visitors to the Region may be able to use the facility as a place to leave their vehicle and access regional transit.

Estimated Cost: \$750,000

Funding: TRPA air quality mitigation funds; County local transportation funds; Caltrans Transportation Enhancement Activities grant; Caltrans Surface Transportation Program; Federal Highways Administration (FHWA) Scenic Byways grant; Private.

4. U.S. 50/California 89 Intersection Improvements and Chain-Up Area

Purpose: Maintain level of service at the intersection and provide a dedicated chain-up area for traffic over nearby Echo Summit. Provide a means for local traffic to avoid delays caused by chain control procedures.

Program Description: Intersection improvements may include traffic channelization, signalization or other traffic control devices. The chain-up area will use existing road shoulders (paved and unpaved) to provide two eleven foot parking lanes approximately 500 feet long, signage, snow storage and drainage facilities. Approximately six feet of additional paving width of compacted road shoulder will be necessary. Overhead lighting is already in place.

Estimated Cost:

Intersection improvements	\$75,000
Chain-up area	\$75,000

Funding: FHWA Scenic Byways grant; TRPA air and water quality mitigation funds; Caltrans Transportation Enhancement Activities grant; private.

5. Recreation Shuttle Service Expansion

Purpose: To reduce vehicle trip ends by providing regularly scheduled shuttle service from Meyers to nearby recreation areas.

Program Description: Recreation areas providing shuttle service such as Heavenly, Kirkwood and Sierra-at-Tahoe would make regularly scheduled stops at the facility. The Beach Bus, Tahoe Queen and casino operators could also extend service to Meyers.

Estimated Cost: \$200,000

Funding: Recreation providers, casinos, TRPA air quality mitigation funds; Private.

SCENIC QUALITY

1. U.S. 50 Corridor Landscape and Lighting Improvements

Purpose: To implement the Meyers community design program and regional scenic quality improvement program along the U.S. 50 corridor, a scenic threshold unit targeted for restoration.

Program Description: Install coordinated landscape frontage and exterior lighting standards on both sides of U.S. 50 in the Yank's Station and West Meyers land use districts. The Meyers Bike Trail landscaping will provide shrubs and groundcover along the highway edge. The adjacent private properties generally would install parking lot landscape screening consisting of trees (mainly deciduous varieties) and selective shrub plantings.

Estimated Cost:

Per typical 100 foot frontage	\$4,000
-------------------------------	---------

Item Cost:

Two area lights, Idaho Wood Products #296 or equivalent	\$1,800
Grading and soil preparation	\$700
Landscape plant materials	\$1,000
Temporary irrigation for plant establishment	\$500

Note: Low-level lighting bollards could be installed for approximately \$1,000 less than the area lights.

Funding: Public and private property owners. Participation in a landscape and lighting benefit assessment district or business improvement district, both permitted under California law, are the recommended methods to implement the program.

2. U.S. 50 Center Landscaped Median

Purpose: To create a sense of arrival to Meyers and the Tahoe Basin; to implement the regional scenic quality program along the U.S. 50 corridor, a scenic threshold unit targeted for restoration.

Program Description: Install an intermittent 14 foot wide landscaped median, including curb and gutter and planting bed using the existing U.S. 50 center turn lane between Apache/Santa Fe and California 89. Total median length is approximately 2,000. It would provide turn pockets and vehicle stacking space at each street intersection and at major driveways serving multiple businesses. The general location of the median is shown the Community Design Plan.

Estimated Cost:
\$30.00 per linear foot \$600,000

Funding: Caltrans Transportation Enhancement Activities grant; FHWA Scenic Byways grant.

3. Gateway Entry Features

Purpose: To create a sense of arrival to Meyers and the Tahoe Basin.

Program Description: Install up to three monuments with landscaping announcing the arrival in Meyers. Locations may include two along U.S. 50 at either end of the plan area and one along California 89 near the plan area boundary.

Estimated Cost:
\$5,000 per entry feature \$15,000

Funding: FHWA Scenic Byways grant; Private.

4. Overhead Utility Lines Converted to Underground Service

Purpose: To create a sense of arrival to Meyers and the Tahoe Basin; to reduce overhead utility lines along designated scenic highway corridors.

Program Description: Convert existing overhead utility lines and overhead service connections to underground service along U.S. 50 within the community plan boundaries from California 89 to the Upper Truckee River, approximately 1,500 linear feet.

Estimated Cost:
\$325.00 per linear foot \$485,000

Funding: El Dorado County undergrounding funds (pursuant to Public Utilities code Rule 20); Private.

RECREATION

1. Permanent Meyers Visitors Center

Purpose: To assist in providing a high quality, outdoor recreational experiences to Tahoe Basin visitors by providing visitor information services at a strategic entry location.

Program Description: Construct a permanent visitors center and interpretive facilities within the community plan area. The plan's preferred location is at the U.S. 50/California intersection. Estimated capacity is 150 person at one time.

Estimated Cost: \$5,000,000

Funding: U.S.D.A. Forest Service, California Tahoe Conservancy, California Department of State Parks.

F. COMMUNITY PLAN REVIEW AND MONITORING

A review of the adopted community plan is required every five years to determine conformance with the plan's approved schedules of development and adequacy of programs, standard, mitigation and monitoring. It may be revised based on the review's conclusions. Monitoring of threshold target attainment and maintenance and capital improvements implementation will occur in accordance with Table 7-1 below. To a large degree the monitoring program relies on existing components of TRPA's monitoring programs, especially activities affecting threshold attainment and maintenance.

EI Dorado County and TRPA[§] will annually review the schedule of plan improvements to ensure the plan is implemented according to schedule. Minor adjustments to the implementation schedule may result from the annual review.

**Table 7-1
Community Plan Threshold Target Monitoring Summary**

Element	Location	Sample Period	Responsible Party
Water Quality and BMP Effectiveness	Representative Projects	Spring runoff and Representative Storm Events	Project Proponents, TRPA
Land Coverage Reduction	Target Restoration Projects	Annually	TRPA
Stream Environment Zone Restoration	Target Restoration Sites	Project Completion Date	TRPA, Tahoe Conservancy, USDA Forest Service
Scenic Resources	Roadway Units #36, #37 and #38; Resource #36-1	5 Year Threshold Review/Evaluation	TRPA, [§] EI Dorado County
Noise (CNEL)	Selected Sites in Plan Area	5 Year Threshold Review/Evaluation	TRPA
Traffic Level of Service (LOS)	Key Intersections (U.S. 50/Cal. 89, U.S. 50/Pioneer	5 Year Threshold Review/Evaluation	TRPA, Caltrans, EI Dorado County
Western Juniper Trees	Existing Locations	Annually	TRPA, [§] EI Dorado County

[§] Amended 12/20/95

Appendix A

COMMUNITY PLAN DESIGN REVIEW GUIDELINES

A. IMPLEMENTING THE DESIGN REVIEW GUIDELINES

1. Community Design Strategy

The Community Plan's design vision for Meyers is to create an attractive, well-designed and organized commercial/public service neighborhood which takes advantage of the outstanding natural setting. As part of an area dependent on tourism, the appearance of each community takes on economic importance. The majority of the plan area lies along U.S. 50 and California 89, both of which are designated state and regional scenic highway corridors. Poorly designed development along the corridor will adversely affect perceptual expectations of people travelling them.

To realize the plan's design vision, a coordinated community design program is included in the plan. Its underlying goals include improving the form and function of existing development and providing clear direction regarding design of new development. Community design will be implemented within the community plan area boundaries by the following elements:

a. **Community Design Goals, Objectives and Special Policies** (excerpted from Chapter 2, Land Use Element).

Community Design Goal: Implement a comprehensive community design program which will improve the visual quality of the commercial area and help Meyers establish a sense of permanence.

Objectives:

- i) Improve the physical appearance of all areas within the plan area. Encourage rehabilitation through the remodeling, upgrading, and aesthetic improvements of buildings, structure and signage.

Policy: All projects which expand or relocate units of use, including land coverage, shall implement or commit to a five year schedule to implement, the landscape improvements fronting its project area. The improvements are described in the Community Plan Design Review. This policy may be waived if the project is in an assessment or improvement district already committed to the improvements.

Policy: Outdoor retail sales and displays visible from U.S. 50 or California 89 shall be consistent with the outdoor display guidelines contained in the Design Review Guidelines. Outdoor storage of items to be sold or rented (not sales displays) shall not be visible from U.S. 50 or California 89.

Policy: Land uses in the U.S. 50 non-operational right-of-way which may be acquired by adjoining property owners shall be limited to parking, decks, paths, signage, landscaping, lighting and water quality control facilities.

- ii) Develop and implement design review guidelines unique to the Meyers area which reinforce the community design goal and establish the “historic Meyers” architectural design theme.

Policy: All projects shall be consistent with applicable sections of the Community Design Plan and Design Review Guidelines (Appendix A). Appropriate guidelines may be added as conditions of approval.

- iii) Develop and implement substitute signs standards to TRPA and El Dorado County sign standards which provide opportunities for visible signage for all uses. Include recommendations for appropriate media and materials. Recognize and respond to the constraints created by the unusually wide U.S. 50 right-of-way and potential snow depths.

Policy: Signage shall be subject to the standards established in the Community Plan Substitute Sign Standards (Appendix B).

- iv) Implement a cooperative sign reduction and consolidation program with Caltrans for signs located within U.S. 50 and California 89 rights-of-way. Remove, reduce and consolidate highway informational and directional signage wherever possible.
- v) Participate in the south shore’s Art in Public Places program. Encourage placement of public art within the community plan area.
- vi) Encourage scenic resource and related community design improvements within the U.S. 50 corridor through implementation of the Pat Lowe Bike Trail.

- b. **Community Design Plan.** A conceptual design improvement plan for the entire community plan area. It indicates private parcel improvements and public design projects to be developed during the twenty year life of the plan. It is found in Chapter 4 of the Community Plan.
- c. **Design Review Guidelines.** Contained in this appendix. The guidelines are a set of recommended design solutions in the areas of highway corridor design, site planning, architecture, exterior lighting and signage. They replace certain sections of TRPA’s Regional Design Review Guidelines (TRPA Chapters 1, 2, 6, 7 and 8). TRPA Chapters 3-5 and 9-11 will remain in effect. Chapter 3, Building Design, advocates use of an historical theme in the design of new remodeled buildings.
- d. **Community Plan Substitute Sign Standards.** Contained in Appendix

B. The substitute standards will replace certain sections of TRPA's Region-wide sign ordinance (TRPA Code of Ordinances, Chapter 20).

- e. **Design Review by the Planning Commission and Planning Department.**[§] The Planning Commission or Planning Department, as applicable, will initially review each development proposal subject to its review to determine its consistency with the Community Plan, including items a. through d. listed above. The zoning ordinance will be amended to separate Planning Commission staff responsibilities, with the staff primarily taking action on minor applications only. The Planning Commission and/or planning staff may recommend and/or add design guidelines to a project as conditions of approval.

2. **Minimum Design Requirements to Qualify for Additional Commercial Floor Area**

To be eligible for consideration of an allocation of additional commercial floor area, each project must incorporate all of the following site planning, architectural design improvements and applicable sign standards listed below under 1. Mandatory Requirements.

Mandatory requirements listed under 1.b. Architectural Building Design, may be substituted by incorporating at least two of the optional guidelines listed in item 2. Optional Requirements, for each mandatory requirement. Commercial floor area allocation procedures area listed in Appendix C.

1. **Mandatory Requirements (all must be met)**
 - a) **Site Planning (Chapter 2)**
 - θ Guideline b. Protection of Western Juniper Tree
 - θ Guideline d. Screening Outdoor Storage and Service Areas
 - θ Guideline f. Highway Landscape Buffer (applies only to parcels or projects areas with frontage along U.S. 50 or California 89)
 - b) **Architecture and Building Design (Chapter 3)**
 - θ Guideline b. Building Form
 - θ Guideline c. Porches
 - θ Guideline d. Building Materials
 - θ Guideline e. Building Colors
 - c) **Signage (TRPA Code Chapter 26, as amended by Meyers substitute sign standards)**
 - θ Section 22.6, General Sign Standards
 - θ Subsection 26.10.A, Building Signs
 - θ Subsection 26.10.B, Freestanding Signs
 - θ Subsection 26.10.D, Directional Signs
2. **Optional Requirements (at least two must be met for each item)**

[§] Amended 12/20/95

substituted under item 1.b)

- a) Highway 50 Corridor and Right-of-Way Site Design (Chapter 1)
 - θ Guideline c. Art in Public Places (can contribute on- or off-site)
 - θ Guideline d. Transit Shelters (can contribute on- or off-site)

- b) Site Planning
 - θ Guideline a. Location of On-Site Parking
 - θ Guideline c. Fencing

- c) Architecture and Building Design (Chapter 3)
 - θ Guideline c. Porches
 - θ Guideline f. Windows

B. MEYERS COMMUNITY PLAN DESIGN REVIEW GUIDELINES

1. U.S. Highway 50 Corridor and Right-of-Way Design

This section provides guidelines unique to the U.S. 50 corridor and right-of-way. Additional guidelines from other sections of this document and TRPA's Design Review Guidelines may be applied based on the nature of the project.

- a) Meyers Bike Trail. Parcels adjacent to the bike trail should implement the site improvements along the property edges contained in the bike trail plans to ensure consistency in terms of access, signage, landscaping, storm water management and other improvements.

- b) Meyers Gateway Entry Statements. Meyers gateway entry statements should be located along U.S. 50 and California 89 near the entrances to Meyers. This may include up to three separate displays. The entry statements should consist of signage, Western juniper trees and boulders reflecting the surrounding Meyers landscape. The entry statements may differ from one another, however, they should present a common design concept

[Graphic #1]

- c) Art in Public Places. Public art is encouraged to be displayed along the Highway 50 corridor on either public or private property in such a manner that all can enjoy. Public art can include sculpture, paintings, street furniture and paving, or landscape design.

- d) Transit Shelters. Transit shelters in Meyers should consist of an enclosed shelter with seating and route signage. Architecture should include textured wood siding and gable-end roof. A stone wainscot may also be used as a design detail.

[Graphic #2]

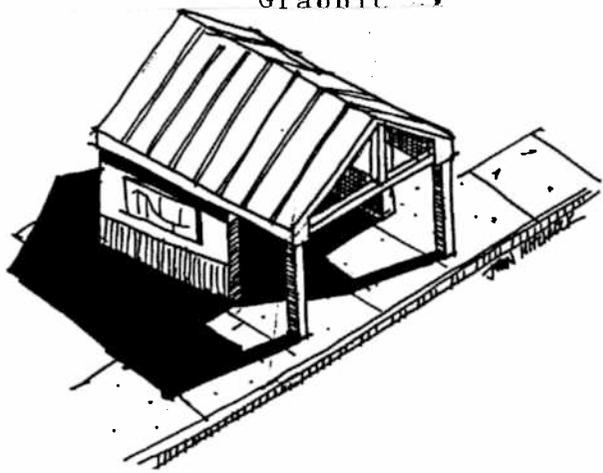
- e) Corridor Street Lighting. Year-round overhead street lighting along the corridor should be done selectively. Lighting should not create a continuous corridor of light. If used, lighting should highlight such things as roadway intersections, driveway entrances, bike trail crossings and the park and ride facility.

The light standard and fixture type used at the Meyers Visitors Center should be the design used along the U.S. 50 corridor. Light standards (poles) and fixtures shall conform to the Scenic Corridor Design Standards established in Chapter 30 of the TRPA Code of Ordinances. All lighting should be directed downward and use cut-off shields or other devices to prevent it from casting light on nearby properties. Light fixture height should not exceed fifteen feet. Low-Level lighting (maximum 8-10 feet tall) may be provided along the bike trail or at



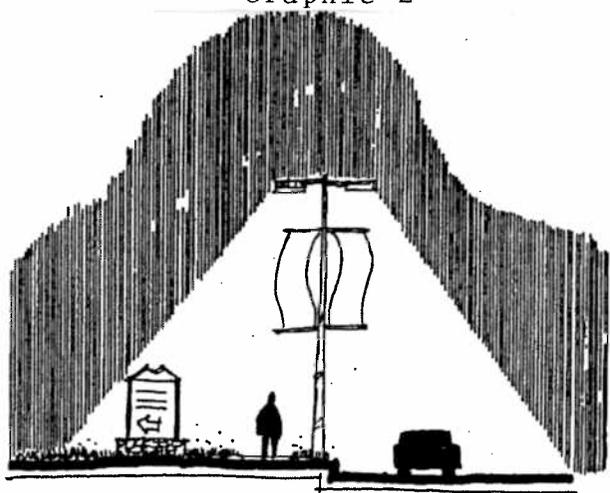
Gateway Entry Statement

Graphic 1



Meyers Transit Shelter

Graphic 2



Use Corridor Lighting to Highlight Special Needs

Graphic 3

gateway entry statements.

[Graphic #3]

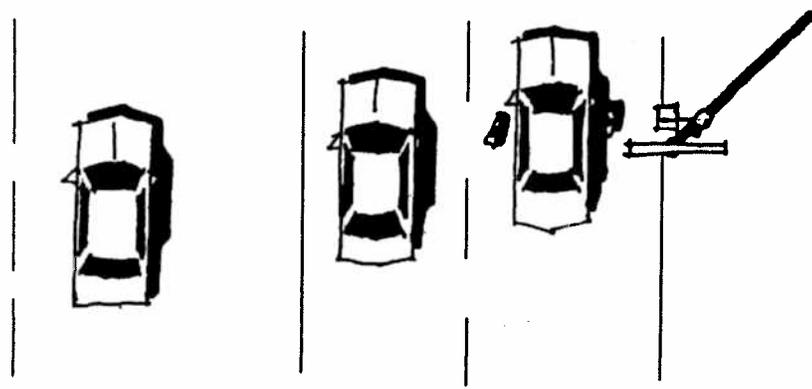
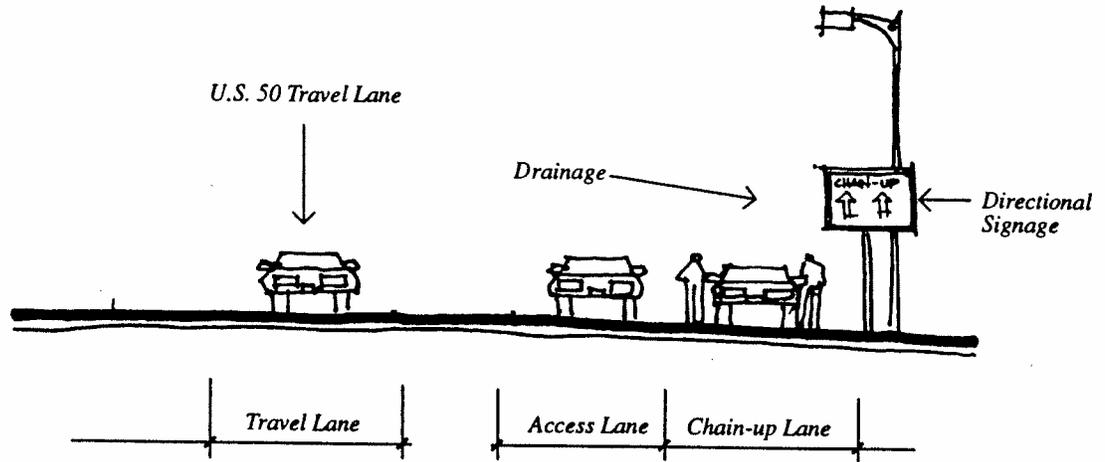
- f) Historic Markers and Displays. In recognition of Meyer's cultural and natural history, historic displays or markers may be erected along the corridor. This includes interpreting the Western juniper tree. They may be combined with other street furniture located at transit shelters, the park and ride facility or along the bike trail. Appropriate media along the corridor include markers, plaques, interpretive panels or signs. The Planning Commission shall approve placement of all historic markers and displays. Refer to Section 2. Site Planning for historic displays on private property.
- g) Highway 50 Chain-up Area. A dedicated chain-up area should be provided for westbound traffic along U.S. 50 west of the California 89 intersection. The area will provide a place to safely pull off the highway to install tire chains. It should include a parking lane, an access lane, drainage, lighting and signage. Chain installers could be provided a reserved space within the area. The Community Park and Ride area may also serve as a chain-up area. Signage should be temporary in nature and could be attached similar to snow poles on existing stakes. Signage should be removed after each chain-up season or no later than May 1.

[Graphic #4]

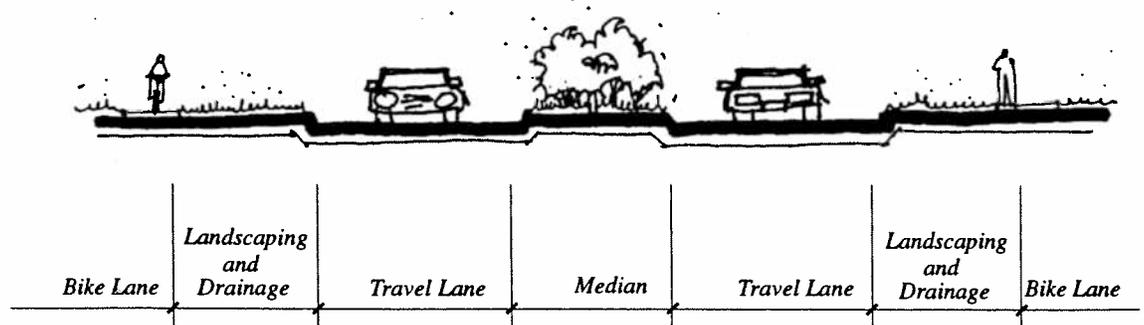
- h) Landscaped Median. A continuous landscape median should be installed in place of the center turn lane along U.S. 50. Approximate locations are shown on the Meyers Community Design Plan. The median should be planted with a mix of low maintenance, native or adapted shrubs and groundcover which break up the highway corridor's apparent width. Left turn pockets and associated vehicle stacking and deceleration space should be provided at key cross street intersections and where combined driveways serve multiple parcels (not one use on multiple parcels). Larger plant materials used in the median should compliment neighboring tree plantings along parcels fronting U.S. 50. The median should include a drainage system to prevent melting snow from running onto the highway and freezing at night.

[Graphic #5]

- i) Right-of-Way Directional Signage. Directional signage located in the public right-of-way should be reduced to an absolute minimum. Non-essential signage should use the Regional off-highway attraction sign face shown below. It may reference another information source such as a radio message or the Visitors Center. Wherever possible, signage should be combined into a single standard or post. This is especially critical at the entrances to Meyers (two along U.S. 50, one along California 89). Caltrans, the California Highway Patrol, TRPA, and EI



Recommended U.S. 50 Chain-up Lane Layout
Graphic 4



U.S. 50 Landscaped Median

Graphic 5

Dorado County should identify excess signage to be removed as soon as possible after it is established.

[Graphic #6]

- j) Agricultural Inspection Station. Due to its functional needs, the Agricultural Inspection Station should be relocated outside the community plan area. Until such time as a relocation is feasible, the inspection station and surrounding parking areas should be redesigned in accordance with the U.S. 50 corridor, site planning, building design, exterior lighting and signage guidelines contained herein.
- k) Snow Removal Along U.S. 50. Caltrans snow removal and storage operations along U.S. 50 should not create a large berm which reduce visibility of signs, driveways and other vehicles. A snow haul should be used following heavy snow fall to move the snow out of the immediate corridor so it does not accumulate over time. Likewise, property owners adjacent to U.S. 50 should not store snow from their site along the highway frontage where it will act to compound the problem.

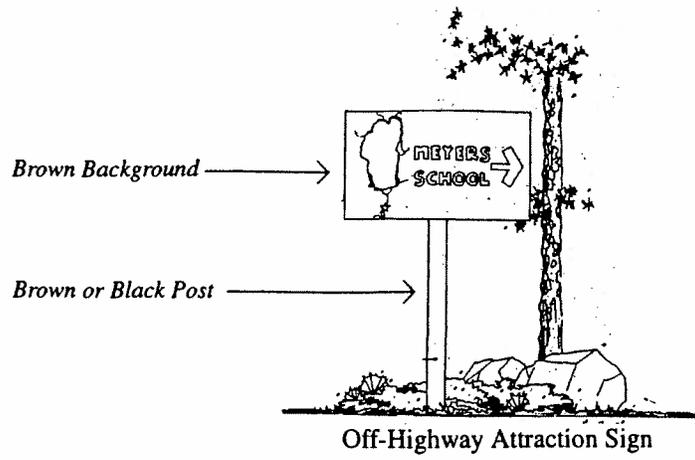
2. Site Planning

- a) Location of On-site Parking. Whenever possible, locate on-site parking areas at the rear of the buildings or otherwise out of sight of U.S. 50 and California 89 (designated scenic highway corridors). This minimize the visual impact of the automobile and keeps the building façade and freestanding sign visible to the highway or street with a minimum of visual interference

Parking should not be located with the U.S. 50 right-of-way nor within the twenty foot front or side setback for parcels abutting U.S. 50 or California 89. These areas should be reserved for landscaping, signage, stormwater management facilities and open space. On-site directional signage can be used to help direct vehicles to parking areas. When parking must be located within the front setback, berms and landscaping should be used to minimize its visual impact. Place as little parking as possible in the front yard setback. Refer to the Landscape Screening guideline for more information.

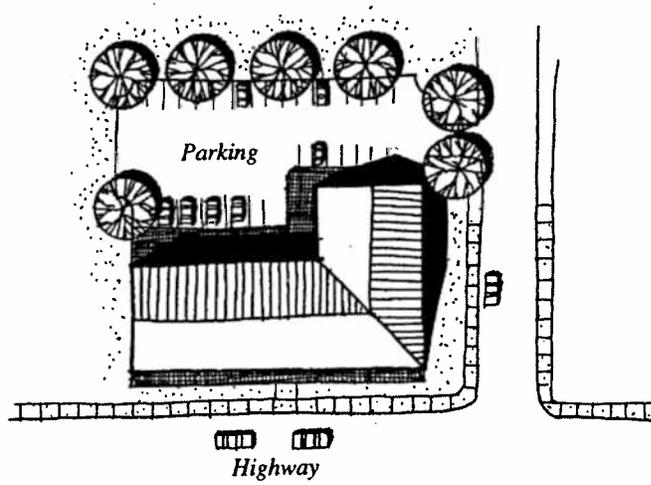
[Graphic #7]

- b) Protection of Western Juniper Trees. Protect and maintain all existing Western junipers through careful site design. Where a juniper appears to be in the way of a proposed building, structure, parking area or other improvement, relocate the improvement in order to save the tree. Do not site improvements in such a manner that a juniper or its root system will be damaged or destroyed. This may result in building a sign, deck or other portion of a building around the tree or incorporating the tree into the structure. Maintain the existing grade around each juniper to a minimum of at least the edge of the tree's dripline. Do not build



Off-Highway Attraction Sign

Graphic 6



Locate On-Site Parking Away From the Scenic Highway Corridor

Graphic 7

structures inside the dripline. Standing dead Western Junipers which are not a safety hazard should be considered for retention as wildlife habitat.

- c) Fencing. Fencing should be constructed of wood, wooden-framed wire, or woodcrete products. Where fencing is used to mark property boundaries, low rail, log or plank fences should be used. Fencing which simply marks property boundaries should not exceed four feet in height. Cyclone or chain link fences which are visible from U.S. 50 or California 89 are not appropriate.

Any fence greater than four feet in height regardless of its purpose should not be located within the twenty (20) yard setback on parcels abutting U.S. 50 or California 89. Any fence greater than four feet in height and fifty feet (50') in length should incorporate trees and/or shrub along it to break up the linear appearance.

- d) Screening Outdoor Storage and Service Areas. Where fencing is needed to screen or enclose an outdoor storage area or service area, a solid wooden fence no greater than six feet in height should be used. Screening is especially important where commercial or light industrial uses border residential uses. Fences taller than six feet should only be used when they are not visible from U.S. 50 or California 89. Fencing greater than fifty feet (50') in length should incorporate trees and/or shrubs along it to break up the linear appearance. When located along U.S. 50 or California 89 solid fencing which is used as a screen should be set back from the property line a minimum of twenty feet to avoid creating the appearance of a wall-off compound.

- e) Landscape Screening. Screening objectionable views using landscape plant materials can be a cost-effective and aesthetically pleasing technique. Landscape screening should generally consist of a mix of trees, shrubs and ground covers which will be effective on a year-round basis (i.e., deciduous trees and shrubs will lose their screening abilities during times they are not in leaf). Landscape screening should be used to screen parking areas, service and storage areas. The size of all plant materials at the time of planting should be such that the screening is effective no later than two years from planting.

Small berms and other minor landforms can be incorporated into the design to provide a visual sense of variety. They can hide an on-site storm water detention basin or trash enclosure. Due to the flat topography found in Meyers (i.e., the Lake Valley landform), berm and other created landforms should generally not exceed three to four feet in height.

The use of native and adapted plant materials is recommended to minimize fertilizer and irrigation requirements, however, using a temporary irrigation system may be used to assist in establishing a new landscape.

f) Highway Landscape Buffers. Parcels with frontage on either U.S. 50 or California 89 should provide landscaped open space along the highway frontage. Depending on site-specific conditions, the buffers may be installed on either public or private land. Each land use districts should use a plant palette of trees, shrubs, groundcovers and planting patterns to produce a similar frontage appearance within the district as described below. Individual projects should install or cause to be installed, the landscape buffer along their property frontage(s).

U.S. 50 as it passes through Meyers is essentially a wide boulevard. Landscape design along the corridor, therefore, should recognize the corridor's function as a boulevard and present a sense of arrival. Larger, deciduous street trees create a sense of organization and order. Existing Western juniper trees help create a sense of place. Together with an understory shrub planting, provided mainly by the bike trail, landscaping will begin to usually unify the corridor.

❑ Yank's Station Land Use District

Landscape buffers should be installed along U.S. 50 and Santa Fe Road/Apache Avenue in the Yank's Station district. The buffer should consist of large deciduous trees and evergreen shrubs. Mature Western junipers trees are not abundant in Yank's Station. Visibility of freestanding signs, driveways and bike trail crossings is an important design determinant. Planting areas and plant material selections should recognize sign visibility needs. A common theme should be developed among the property owners with U.S. 50 frontage.

❑ West Meyers, Lake Valley and Upper Truckee River Land Use Districts

Landscape buffers should be installed along parcels adjacent to U.S. 50 and California 89 in the West Meyers, Lake Valley and Upper Truckee River districts. The districts represent a gateway to the Tahoe Basin and should present a coordinated and well-landscaped image. A common theme to be implemented by all property owners with U.S. 50 frontage should include large deciduous trees and the existing Western juniper trees.

❑ Industrial Tract

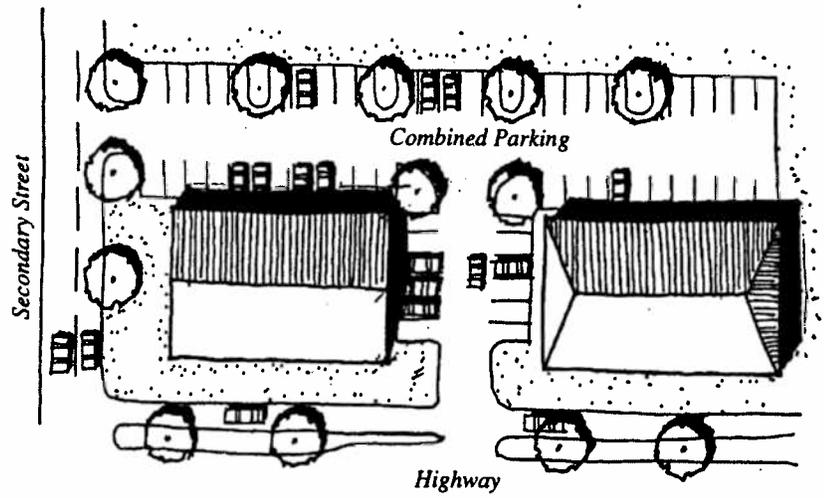
Landscape buffers should be used along the California 89 corridor to provide screening of the Industrial Tract. Existing native vegetation already provides an excellent screen. It should be maintained across all parcels. Any additional landscaping should compliment the existing native vegetation.

g) Historic Markers and Displays. In recognition of Meyer's cultural and natural history, historic displays or markers may be erected at or near the site of historic features, events or places. This includes interpreting the Western juniper tree. Appropriate media include photographs,



Use A Single Item to Communicate What is For Sale or For Rent

Graphic 8



Use Combined Driveways Wherever Possible Along U.S. 50 and California 89.

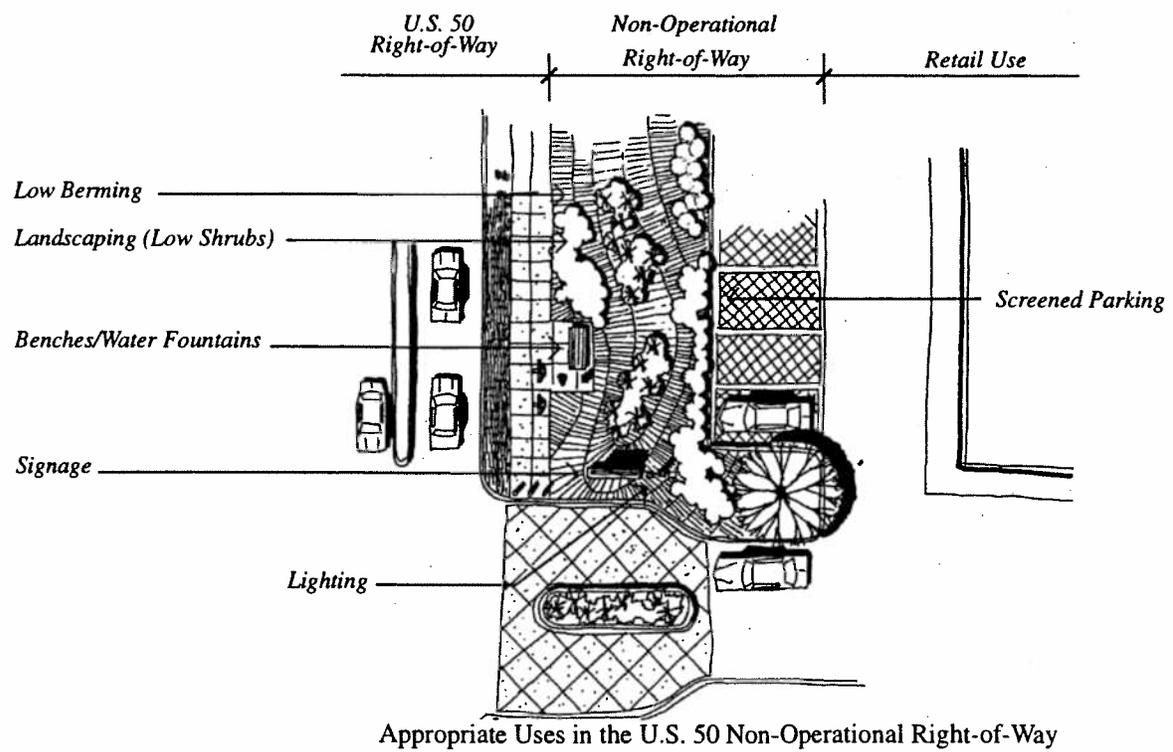
Graphic 9

display cases, markers, plaques, interpretive panels or signs. The Planning Commission shall approve placement of all historic markers and displays.

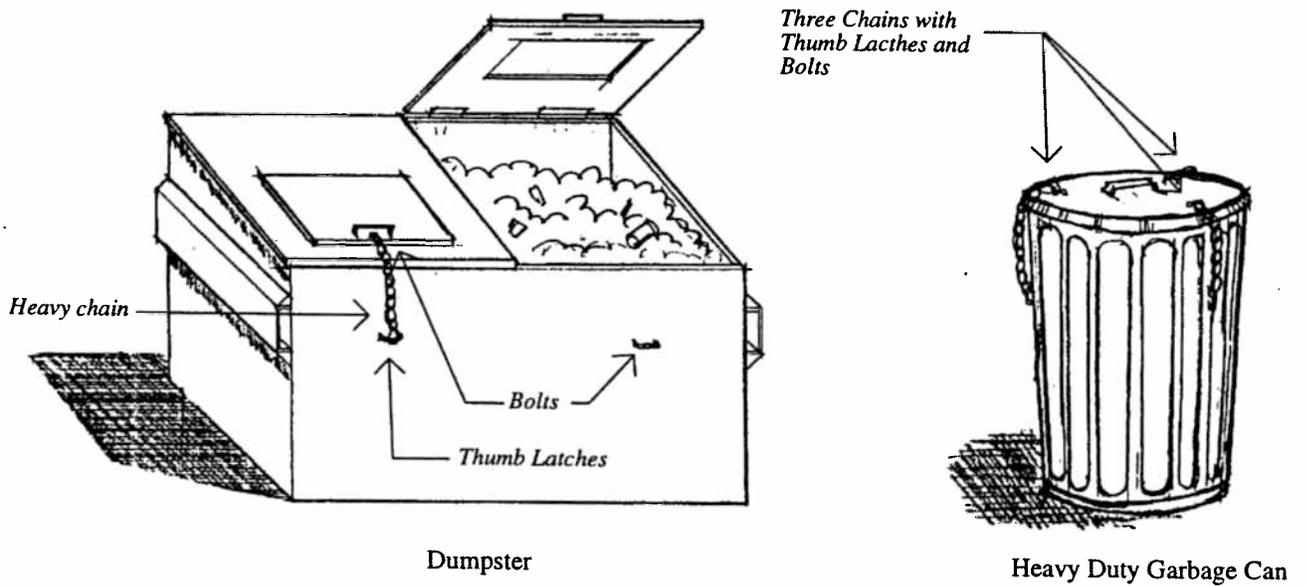
- h) Outdoor Retail and Rental Displays. Outdoor display can be visually distracting and present a cluttered image of Meyers. Outdoor displays of retail and/or rental merchandise should be minimal and easy to comprehend when viewed from the roadways or bike trail. This applies to items which are not customary and incidental to the use such as recreation equipment rentals, rugs, cars or clothing. A single item showing what the business rents or sells should be used whenever possible. It should be integrated into a display which may include signage. Rotate the item or items by season if needed. Include snow removal considerations in the design and location of the display. Outdoor displays which are not visible from the roadways or bike trails may use greater amounts of display area or items than those visible from the roadways.

[Graphic #8]

- i) Combined Driveways Along U.S. 50. Whenever possible, driveways on adjoining parcels along U.S. 50 should be combined to form a shared access point. Combined driveways should always be used when a back street or drive provides a second point of site access. Individual driveways which are combined may then be removed and restored. Combined access points reduce traffic friction on the highway, convey an image that land uses are planned together, and can provide increased safety for bike path users who must cross the driveways. Shared driveways are encouraged to provide landscaped medians in the driveways. Figure 3-1, Transportation and Circulation, identifies areas along U.S. 50 which are priority targets for combining driveways.
- j) Use of Former U.S. 50 Non-Operational Right-of Way. The forty foot wide former U.S. 50 non-operational right-of-way strips of land located primarily in West Meyers plays an important role as an edge between adjacent private lands and the public highway corridor. It acts as an extension of the existing parcel. The way in which the land is used, therefore, is critical to successfully solving the edge relationship. Appropriate land uses include vehicle access, signage, transit shelters, landscaping paths and walks (pedestrian and bicycle access), outdoor seating (i.e., patios, benches and decks), lighting, screened parking and water quality protection facilities. Buildings should not be located in this area. Refer to Chapter 2 for the specific policy regarding uses.
- k) Provide Bicycle Racks. Bicycle racks should be provided at all uses which attract bicyclist. Racks or other secure facilities for bicycle storage should be provided within each land use district. Retail and service commercial uses, transit and park and ride facilities and recreation uses are primary uses in Meyers which generally attract bicyclists. Share racks or other storage methods are appropriate for



Graphic 10



Graphic 11

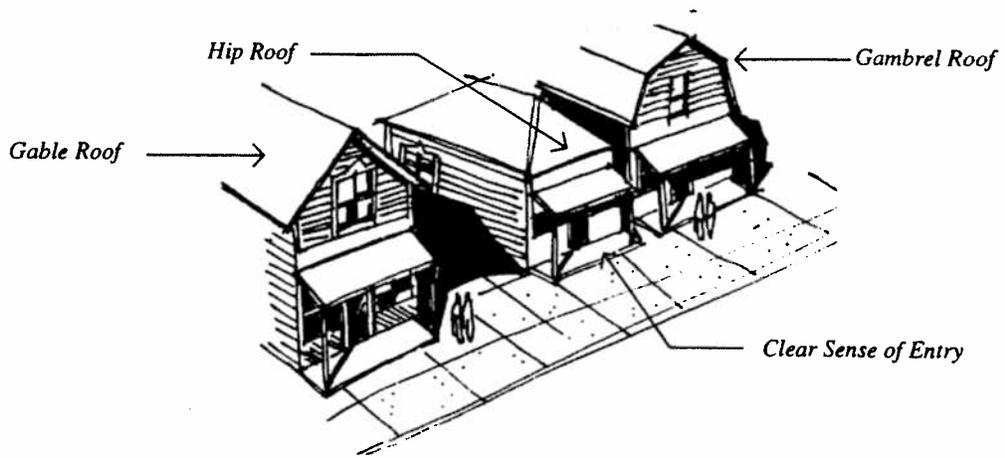
multiple uses within a single project or development.

- l) Provide Bear-Proof Trash Facilities. Trash and garbage facilities on all uses should be secure from bears. For residential uses, garbage should be cleaned up and made unavailable to bears. For all other uses, garbage cans and dumpsters should be “bear-proofed” with metal lids that are latched with a minimum of two latches. Example are shown below

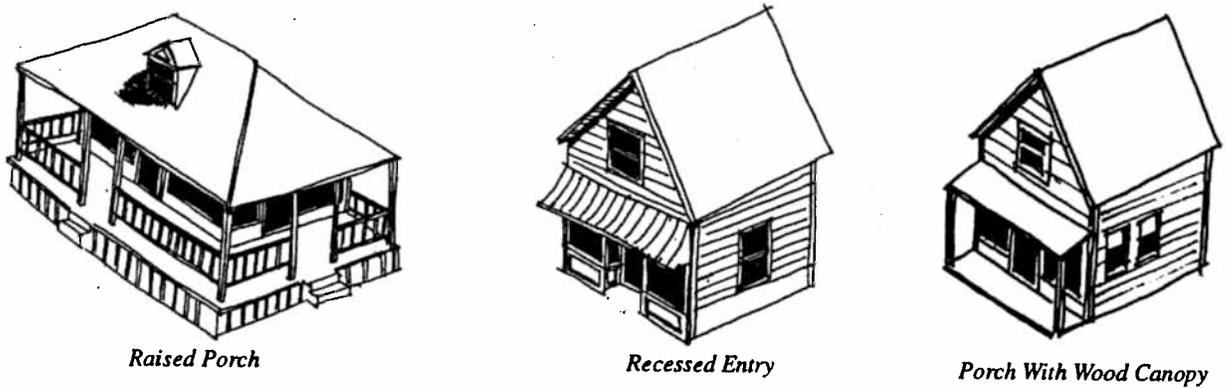
[Graphic #11]

3. Building Design

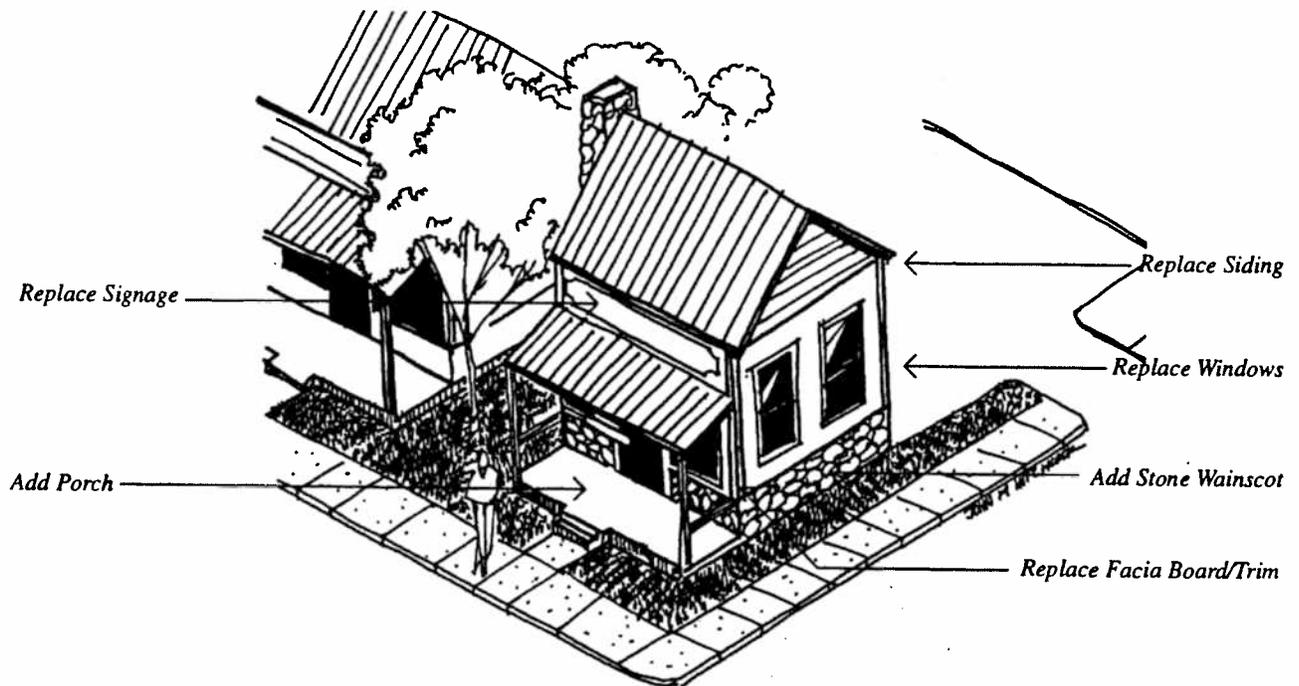
- a) Architectural Theme. A consistent architectural theme is a powerful means to create and convey a sense of place. This does not mean that all buildings will or should look alike. It is the use of variations on a theme which can be an asset to the community. Historically, the commercial area was used as a way station. The historic architectural design theme which uses modern building technology is the preferred theme for both new and redeveloping buildings. The following architectural elements should be used to convey the historic theme.
- b) Building Form. Buildings should generally use steeply-sloping gable end or gambrel roofs with pitches of 6:12 or greater. Buildings with hip roofs are also appropriate. Second story dormers are generally consistent with the historic theme and may be used. Flat-roofed buildings are not appropriate. Buildings should have a clear sense of entry. Avoid excessive ornamentation. [Graphic #12]
- c) Porches. Covered front porches which run the entire length of the building facade are a simple design element which helps to communicate the historic theme. The porch may be recessed in the form of a mudroom/vestibule or may extend outside of the building envelope covered by a separate shed roof. [Graphic #13]
- d) Building Materials. Building materials should consist of wood and/or natural stone sidings. Dimensional concrete products which look like wood may be used. Appropriate siding materials include cedar, half-round log, or horizontal tongue and groove. Wood shingle siding may be used. Vertical cedar or redwood board and batten sidings may be used when battens are carried to the roofline on all sides, including the gable ends. Plywood sidings and stucco are not appropriate. Roof materials should be shingles (black is preferred), metal roofing, or treated fire retardant shakes.
- e) Building Colors. The process of selecting colors for building siding, trim and roofing must consider the building materials and the architectural style. All elements are seen together. Certain combinations work together to produce a pleasing appearance while other combinations work against each other. The following colors and materials



Recommended Building Forms
Graphic 12



Covered Front Porch Options
Graphic 13



Redeveloping An Existing Building Using the Historic Design Theme
Graphic 14

combinations are recommended:

- ❑ Historic Theme Buildings
Siding colors should be beige to umber brown tones, mossy green tones, or white. Roofing should be brown tones, mossy green tones or black.
- ❑ Non Historic Theme Buildings (Those buildings which do not use building form and materials guidelines set forth above.)
Siding colors should be beige to umber brown tones, or mossy green tones. Roofing should be beige to umber brown tones, mossy green tones or black. White is not appropriate for non-historic them buildings.

Buildings should generally be stained, however, painting is acceptable. Accent colors which fall outside the recommended building siding colors should be used sparingly and should be limited to trim, fascias and architectural details such as window frames, door frames, shudders, planter boxes, railings and balusters.

- f) Windows. Windows should be individual wooden windows oriented vertically. Windows should have mullions, a sash and a sill. Exterior shudders and planter boxes located underneath the window may also be used. Double hung windows are acceptable.
- g) Redeveloped Existing Buildings. Redeveloping existing buildings with one or more of the architectural features listed above can begin to create a sense of place incrementally. Some design features are more feasible to retrofit than others. The following improvements are recommended in order of importance to achieve the historic them:
 - i) Remodel building façade(s) facing U.S. 50 and California 89;
 - ii) Add porch or vestibule;
 - iii) Repaint/restain with recommended colors;
 - iv) Replace siding;
 - v) Replace signage;
 - vi) Replace windows and doors;
 - vii) Replace fascia board and trim;
 - viii) Add stone wainscot; and
 - ix) Add window boxes.

Installing the improvements should first be done on the façade(s) which face Highway 50 or Highway 89. The sides should be done next followed by the rear. [Graphic 14]

- h) New Buildings. New buildings can easily establish the preferred architectural character from the beginning. New commercial buildings in Meyers should utilize the Building Design guidelines listed above to create the historic architectural theme.

- i) Public Service Buildings. Meyers is home to several public service uses. Several new public buildings, including the interagency visitors center, are planned within Meyers. As public spaces, these buildings have a responsibility to draw interest and convey the community theme. New public service buildings should utilize the Building Design guidelines listed above to create the historic architectural theme.
- j) Provide Usable Outdoor Spaces. The process of building design and site planning should consider the types of outdoor spaces that a development will create. Projects should create a positive outdoor space(s) that has identity and function due to its sense of enclosure and orientation. Outdoor spaces can be used for informal seating, meeting and gathering, to create an outdoor café or to provide relief from the weather and other influences. Projects incorporating retail uses should strive to include outdoor space(s) as part of their program.

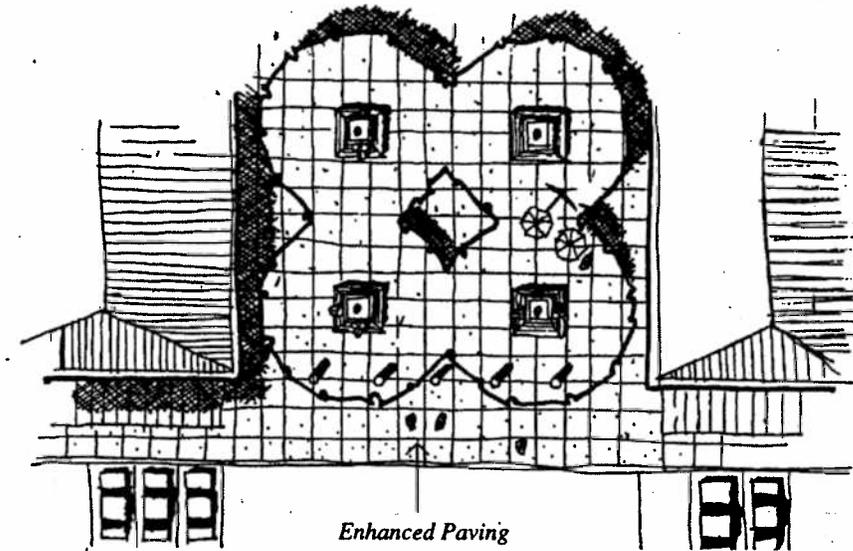
[Graphic #15]

- k) Designing for Snow. Building design and orientation should recognize and respond to the presence of snow. Understanding a site's microclimate in terms of sunny and shaded areas, prevailing winds, and areas which accumulate drifting snow will help successfully solve the design problem. The following guidelines are recommended:
 - i) Locate building entrances under the gable end of pitched roofs.
 - ii) Avoid locating entrances and walkways under roof eaves or other surfaces which can be expected to unload snow on unsuspecting persons below.
 - iii) Provide cover for stairways and other entrances. Porches can be a successful form of cover.
 - iv) Avoid locating stairs under the dripline of roof eaves.
 - v) Avoid extending balconies beyond the roof eaves.
 - vi) Provide covered walkways between buildings using a compatible architectural style.

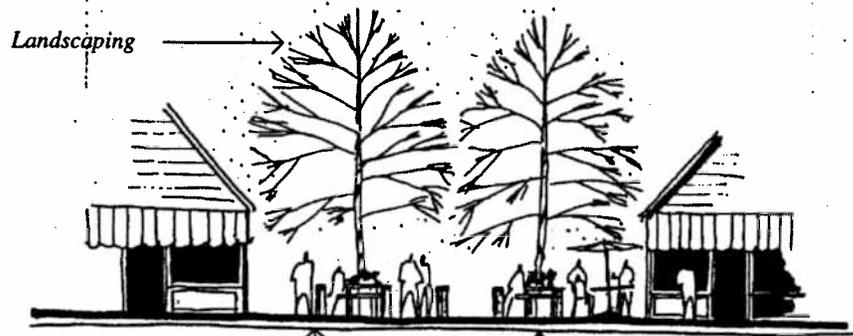
4. Exterior Lighting

- a) Use of Exterior Lighting. Exterior lighting should be used selectively to provide light for functional needs such as transit stops, building entries, walkways, paths, building signage, public art, grade changes and safety. Human-scaled lighting for outdoor gathering spaces may occur, provided the lighting is of low intensity and does not cast light off-site. General area lighting of landscaped or other outdoor areas where people are not expected to gather or traverse should not be done. All lighting should generally be directed downward and should not cast light off the property. All light sources should be concealed from view. Exterior lighting should not be used to illuminate entire buildings.
- b) Light Fixture Styles. Exterior light fixtures should be of a common design style(s) throughout Meyers. Recommended styles for

THIS



Enhanced Paving



Landscaping

Bollards

Benches

NOT THIS



Provide Usable Outdoor Spaces

Graphic 15

freestanding lights are shown below. The Idaho Wood model is found at the existing Meyers Visitor Center. The recommended wall-mounted fixture is used at Carlos Murphy's Restaurant in South Lake Tahoe. Additional designs using forms or materials from other design elements which represent the historic theme may also be acceptable.

[Graphic #16 & #17]

- c) Low-Level Bollards. Low-level bollards may be used instead of overhead lighting for pedestrian paths or along the bike trail. Two bollards designs are recommended. Idaho Wood Products models R276, 277 or 279, or equals provide a design consistent with the recommended overhead fixtures. An alternative is a custom-fabricated bollard which consists of approximately five logs standing on end and banded together with steel straps. The light source is concealed inside the center of the logs.

[Graphic #18]

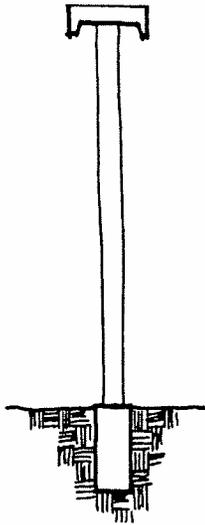
- d) Landscape Lighting. Exterior landscape lighting should be used selectively to highlight pathways or special landscape plantings. It should not illuminate an entire landscape bed or planting area (especially turfgrass). Where lighting of plant materials is desired, the light source should be concealed from view, produce a low level of illumination and used on only a small number (one to three) of displays.
- e) Light Fixture Height. Maximum height for parking area light standards should not exceed 15 feet. For walkways, including the bike trail, the maximum height should not exceed 10-12 feet. When used as bollards, the height should not exceed 4-5 feet. Building-mounted fixtures should not exceed 8-10 feet in height, except when lighting a building sign. Building sign lighting should not be located any higher than one to two feet from the top of the sign.

5. Signage

- a) Freestanding Signs. Freestanding signs should be set on a monument base which is stone or has a natural stone veneer. The sign face should be constructed of metal, concrete, wood, or sign foam. Plastic or plex faced signs should not be used. Neon tubing should be used sparingly as an accent, or not at all. Reflective surfaces on signs are inappropriate and not permitted by the substitute standards.

Freestanding signs should not be placed in areas which will be obscured from vision by stored or plowed snow. If necessary, locate or relocate the sign in places where any accumulated snow can easily be cleared. A snow storage plan may be needed to optimize snow plowing operations, snow storage areas and sign visibility.

[Graphic #19]



Freestanding Light Standard
(Found at Meyers Visitors Center)

Specifications:

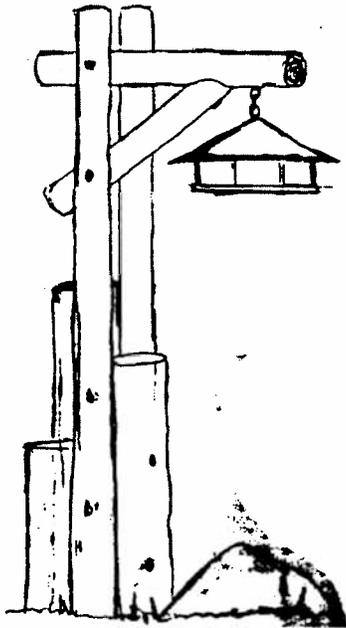
- Idaho Wood Model #296/296D or equal.
- Steel super structure the entire length.
- Head: Boxed with 3/4" cedar.
- Post: Lock mitered octagon cedar.
- Finish clear.
- Luminaire: 150 watts high pressure sodium
- Initial lamp lumens: 16,000



Wall Mount Light Fixture
Specifications:

- Benjamin Exterior Lamp Shade
- Model # V-7642 or equal.
- Pendant mount
- 150 Watts incandescent
- 12" high by 14" diameter
- Forest green finish

Graphic 16

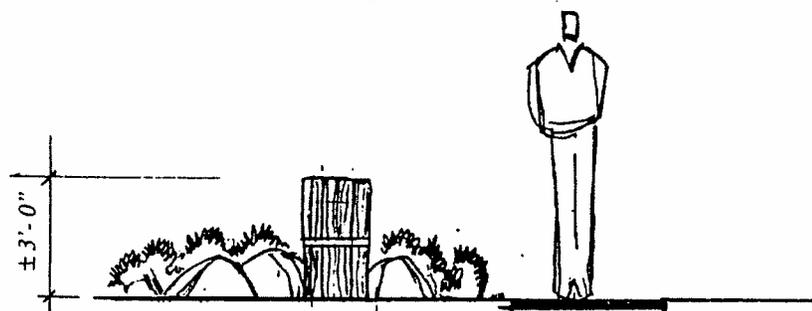


Street and Log Light Fixture Specifications:

- Vertical Standards: 2 7" - 9" dia. debarked cedar or lodgepole logs. 10' - 12' tall.
- Cross Arm and Brace: 5" - 6" dia. debarked cedar lodgepole logs, approximately 5' long, extend behind vertical standard.
- Lumination Source: 1 - 3 incandescent bulbs, 150-200 watt maximum per bulb. Place conduit along interior side of vertical standard.
- Fixture: Rusted steel lantern style fixture, square or multi-sided with glass insert panels, fixture top cover should overhang glass panes approximately 2" - 2 1/2".
- Color/Finish: Natural color, finish with wood preservative or linseed oil.
- Fasteners: Threaded bolts, washers and nuts, black or brown color to match fixture.
- Ground Connections: Set in steel column holder or equivalent to prevent rotting.

Steel and Log Light Fixture

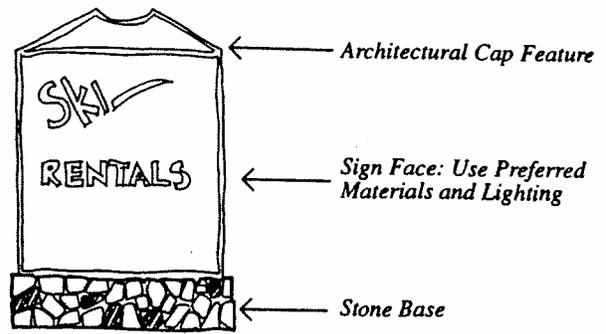
Graphic 17



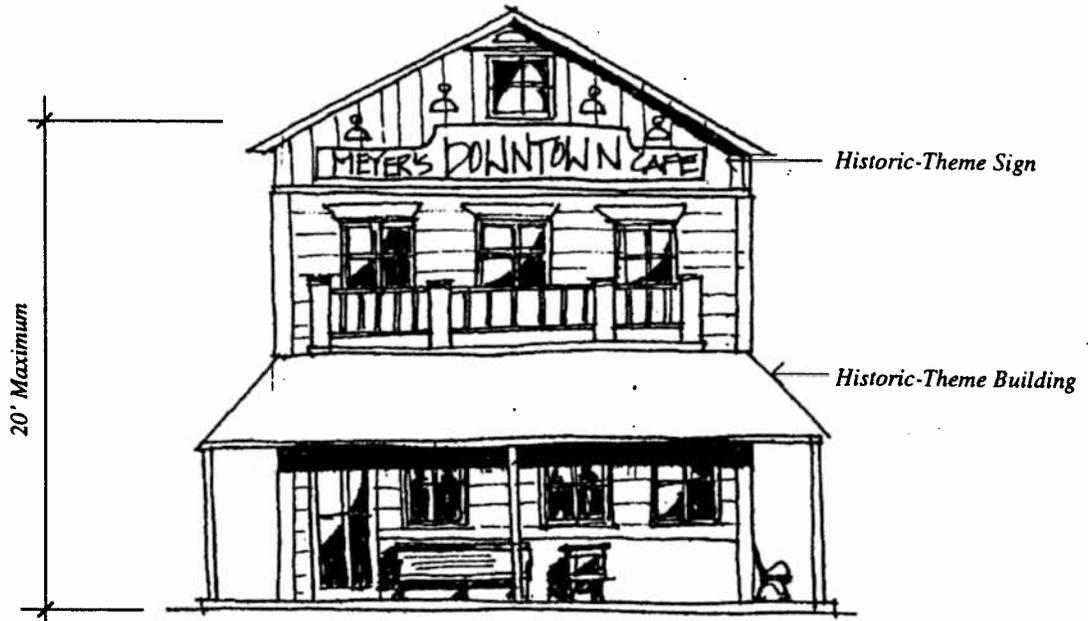
Low-Level Bollard Light

Graphic 18

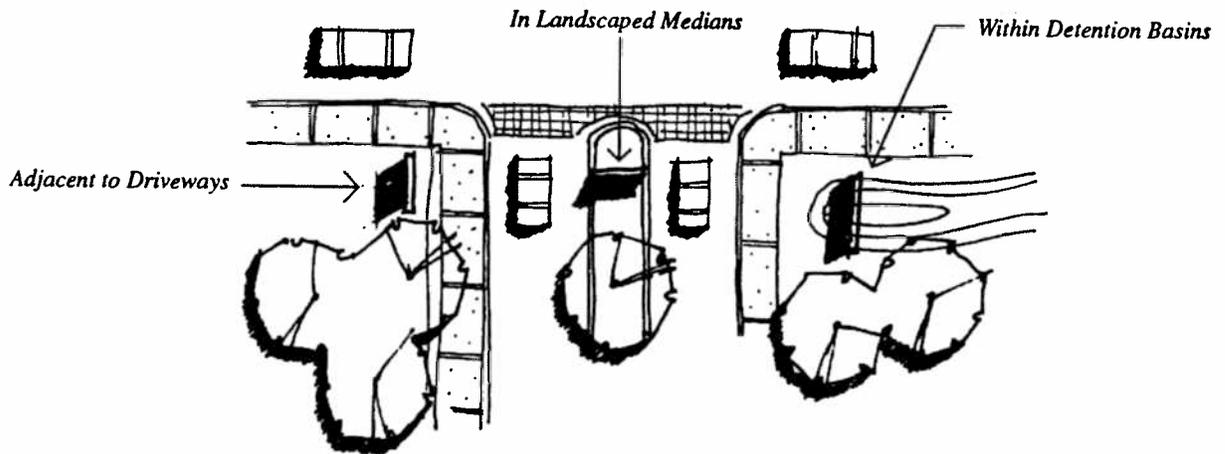
- b) Building Signs. Building signs should be constructed of metal, concrete, wood, or sign foam. Individual or channel-lume letters may be used. Plastic or plex faced signs should not be used. Neon tubing should be used sparingly as an accent, or not at all. Building signs should compliment the building's architecture and should be integrated into its design. Reflective surfaces on signs are inappropriate and not permitted by the substitute standards.
- c) Building Signs on Historic Buildings. Historically, building signs were often painted directly on the wall surface. This is an appropriate technique only when using the historical building design forms and materials (building utilizing Building Design guideline 3.b through 3.f inclusive). Similarly, the Meyers substitute sign standards provide for wall signs on the historical theme buildings to be located above a line formed by the top of the second floor windows or twenty feet. Refer to the substitute sign standards. [Graphic #20]
- d) Freestanding Sign Location and Snow Removal. Given the amount of snow which falls in Meyers, special attention should be paid to the location of freestanding signs. Generally, freestanding signs should be located in places where they can easily be cleared of snow. Recommended locations include adjacent to driveways, within driveway landscaped medians or near walkways. They should be placed near an area which is regularly shoveled so area around the sign can also be shoveled. Two additional recommended locations area: on an island within a storm water detention basin (use negative space to ensure visibility); or set far enough away from snow storage areas to remain visible (e.g., Tahoe Paradise Winter Sports Center sign). If used, detention basins should be adequately sized to account for the change in storage volume taken up by the island. Snow storage areas should not be located in front of freestanding signs where the sign's visibility would be impaired.
- [Graphic #21]
- e) Sign Lighting. Signs should generally be externally lit from above the tip of the sign. External light sources should light the sign only and should incorporate a cone or cutoff-type shield to direct the light beam at the sign.
- Internally illuminated signs are not recommended. When used, only the lettering should be illuminated using backlighting in a manner similar to the Inn By the Lake sign in South Lake Tahoe.
- f) Sign Face Layout. Signs should have no more than 60% of the sign area in copy. Sign copy includes all letters, numbers, characters, symbols and other graphics which are part of the sign. Avoid billboard-style layouts which generally try to provide too much text to be easily



Freestanding Sign
Graphic 19



Use Historical Building Signs to Compliment Architectural Design
Graphic 20

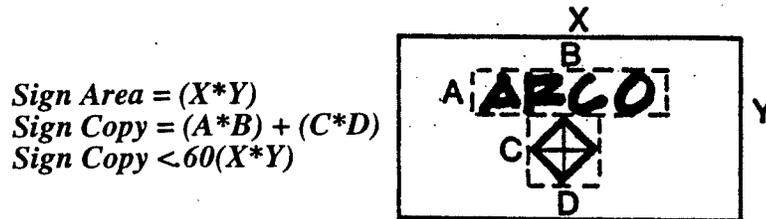


Recommended Sign Locations
Graphic 21

read. This guideline generally does not apply to signs which consist of individual letters, characters, or other symbols and which have no perimeter or border.

[Graphic #22]

- g) Develop a Coordinated Sign Plan for Multiple-Tenant Complexes. Multiple-tenant buildings and complexes should develop a coordinated sign program that minimizes the potential visual conflicts and competition among tenant signs, yet insures adequate identification for each tenant. Freestanding signs used to identify such complexes which are seen by motorists on U.S. 50 should strive for simple wording and layout rather than too much small text which cannot be read. Tenant identification should be provided by building or projecting signs within the complex.



Recommended Sign Face Layout

Graphic 22

C. DESIGNERS, CONTRACTORS, AND MATERIALS SOURCES

Disclaimer: The following list of services and suppliers is provided as a aid to the user. It is not an endorsement of any of the businesses listed herein. This list is not intended to be all-inclusive. Project proponents are not required to use the services or suppliers from the list.

- θ **Architectural Design and Engineering**
 - Fagan & Crouse, Architectural Design (530) 541-3520
 - John Fellows, Sunrunner Studios (503) 577-8255
 - K.B. Foster, Engineering (530) 546-3381
 - Haen Engineering (530) 541-4343
 - James Jones, Civil Engineer (530) 544-2110
 - Jensen-Klein Architect (530) 541-6115
 - Birger Lundbert, Architect (530) 542-0660
 - Robert Mason, Architect (530) 544-2653
 - William F. Pillsbury, Civil Engineer (530) 541-1694

- θ **Signage and Dimensional Signs including Wood, Metals, Foam, Concrete and Ceramic**
 - Banana Graphics (530) 541-0190
 - Columbine Designs (530) 577-5915
 - Dollar Signs (530) 544-7058
 - Frog Eye Productions (530) 577-3697
 - Ron Ramsey, Woodcarver, Nevada City (530) 265-5052
 - Signs of Tahoe (530) 544-0265
 - Sunrunner Studios (530) 577-8255
 - The Studios, Carson City, Nevada (702) 588-2990

- θ **Landscape Design, Soil and Erosion Control Consultants**
 - Julie Etra, Western Botanical Services, Reno, NV (702) 322-5011
 - Acanthus, Sacramento, CA (916) 444-9020
 - Sierra Land Design, Placerville, CA (530) 622-0560
 - L. T. Kellogg, Carson City, NV (702) 882-7865
 - Kelly Erosion Control and Landscaping, Truckee, CA (530) 587-8875

- θ **Landscape Materials**
 - Amacker Construction Supply (530) 544-2039
 - Alpenglow Garden Center (530) 577-8255
 - Sunbasin Landscape and Nursery, Stateline, NV (702) 588-8326
 - Supply One, Meyers, CA (530) 573-0800
 - Tahoe Sands and Gravel (530) 541-6862
 - Tahoe Tree Company, Tahoe City, CA (530) 582-3911

- θ **Log Structures**
 - A Place In The Sun Log Homes, Inc., Timber, OR (503) 324-0922
 - Oregon Log Homes, Sisters, OR (503) 549-9354
 - Real Log Homes, Dayton, NV (702) 246-0590
 - Rocky Mt. Log Homes, Hamilton, MT (406) 363-5680

- θ **Native Plants**

 - Alpenglow Garden Center (530) 577-8255
 - Sunbasin Landscape and Nursery (702) 588-8326
 - Julie Etra, Western Botanical Services (702) 322-5011
 - Nel's Garden Center (530) 541-2552
 - Tahoe Tree Company, Tahoe City, CA (530) 583-3911
 - Watermelon Patch, Tahoe City, CA (530) 583-2611
 - Zehren's Landscape Nursery (530) 544-2622

- θ **Metal Roofs**

 - Meeks Building Supply, South Lake Tahoe, CA (530) 544-2237
 - Supply One, Meyers, CA (530) 573-8800

- θ **Stone and Rock**

 - Bing Material, Gardnerville, NV (702) 265-3641
 - South Tahoe Block Company (530) 541-3077
 - Sunrunner Construction (530) 577-8255
 - Tahoe Sand and Gravel (530) 541-6862
 - Teichert Construction, Sacramento, CA (916) 386-6800

- θ **Stone Masons**

 - B.W. Masonry, Stateline, NV (702) 588-3672
 - Michael Glover (530) 577-3130
 - Stuart Jenkins (530) 544-2594
 - Sunrunner Construction (530) 577-8255
 - Ken Sailing, Tahoe City, CA (530) 581-2237
 - Ted Wendell (530) 544-7354

- θ **Wood Siding, Shingles, Trim, Fencing and Decking**

 - California Redwood Molding Company, Sacramento, CA (916) 381-8487
 - Hedland Lumber (Cedar Specialists), Sacramento, CA (916) 331-6611
 - Meeks Building Center (530) 544-2237
 - Sticks & Stones, Carson City, NV (702) 882-3133
 - Supply One, Meyers, CA (530) 573-8800
 - C&D Lumber Company, Riddle, OR (503) 874-2281
 - Neilsen & Ferrari Lumber Co., El Dorado, CA (530) 622-9211
 - (Provides cut lumber that would not require a grade stamp. A lot of lead time would be needed.)

- θ **Windows**

 - Meeks Building Center (530) 544-2237
 - Ponderosa Glass (530) 541-2800
 - South Shore Glass and Door (530) 541-2691
 - Silver State Screening, Reno, NV (702) 323-4224

Appendix B

SUBSTITUTE SIGN STANDARDS

The following sign standards replace certain sections of El Dorado County's Ordinance Code, Chapter 17 and TRPA's Code of Ordinances, Chapter 26, Signs. The substitute standards recognize and respond to unique situations in Meyers including, but not limited to, the unusually wide public highway right-of-way, existing building setbacks from U.S. 50, heavy snowfall and snow storage practices.

COMMUNITY PLAN SUBSTITUTE SIGN STANDARDS

1. Increase in Maximum Allowed Sign Area. Sign area for building and free standing signs, which are visible from highways with a posted speed limit of 40 miles per hour or greater, may be allowed up to 20 percent additional sign area over the maximum allowable area for each sign as calculated based on the applicable provisions of Chapter 26 as amended by the Meyers Community Plan [replaces subsection 26.6.Q, Increases in Maximum Allowed Sign Area].
2. Increases in Maximum Allowable Building Sign Height. Maximum height of building signs which are painted on the building or appear to be painted on the building may be equal to a line formed by the top of the second floor windows or twenty feet when the building is determined to conform to the Meyers historic architectural design theme. This includes signs which are vertically oriented. This provision does not include signs which are internally illuminated, including neon [amends subsection 26.10.A, Building Signs in Commercial/Public Service Plan Areas].
3. Temporary Signs for Winter. Until such time as Caltrans provides a snow haul for plowed snow stored along U.S. 50, businesses located adjacent to U.S. 50 may install temporary signs which meet the design criteria listed below provided an annual permit for the sign is obtained. Once the snow haul has begun, no temporary signs will be permitted and the temporary sign provision will be deleted from the Community Plan and substitute sign standards [replaces subsection 26.6.L, Portable Sign Prohibition].

Design Criteria for Temporary Winter Signs:

- A. Each business located adjacent to U.S. 50 may install one temporary sign when plowed snow along U.S. 50 obscures the visibility of a legally existing freestanding or building sign;
- B. The sign shall be placed on private property and may be placed in such a manner as to be visible to motorists on U.S. 50;
- C. The sign shall be a maximum of 12 square feet in area;
- D. The sign shall be constructed of sign board, plywood or sign foam only. No other materials are permitted including, but not limited to, such materials as cloth, banners or flags;

- E. The sign shall have a dark background;
 - F. The sign shall not be internally illuminated;
 - G. No other temporary signs shall be in place; and
 - H. The temporary sign shall be removed once the permanent sign face is completely visible from U.S. 50.
4. Replacement of Existing Non-Conforming Signs. Existing non-conforming signs shall be brought into conformance with the sign standards, as amended by the Meyers Community Plan, if a sign is altered structurally, or if the sign face is altered, except for maintenance [amends subsection 26.14.C(4), Removal of Non-Conforming Signs].
5. Transfer of Certain Building Sign Area. Building sign area which may otherwise be permitted under this chapter may be transferred from the building frontage against which the permissible sign area is determined to another side of the same building which does not have a building frontage as defined by TRPA's Code of Ordinances provided the following conditions are met [amends subsection 26.10.A]:
- A. The building on which the signage is to be transferred is determined to conform to the Meyers historic architectural design theme;
 - B. The sign area is completely transferred such that no signage remains or is installed on the frontage from which the sign area was transferred;
 - C. The building side receiving the transferred sign area is the side adjacent to the building frontage from which the sign area is transferred;
 - D. The transfer may occur one time per primary use;
 - E. The transferred sign area is not internally illuminated, is not used in a projecting sign and is found to be consistent with the Meyers Community Plan Design Review Guidelines for Building Signs on Historic Theme Buildings; and
 - F. The transfer to the building side receiving the transferred area does not result in a total building sign area greater than 72 square feet, regardless of sign ownership or arrangement of uses within the building.
6. Prohibition of Reflective Materials. No sign shall use or include reflective materials on any part of the sign or sign structure [amends subsection 26.6.F, Prohibited Devices].

Appendix C

COMMERCIAL FLOOR AREA ALLOCATION PROCEDURES

A. INTRODUCTION

The Meyers Community Plan contains specific procedures by which additional commercial floor area will be allocated. Chapter 2, Land Use, contains economic development policies which state:

Policy: Projects seeking allocations of additional commercial floor area within the plan area shall be subject to the plan's Commercial Floor Area Allocation Procedures. TRPA and El Dorado County shall only consider applications for those projects utilizing allocated floor space, when they have received a recommended floor space allocation from the Board of Supervisors or their designee.

Policy: Allocation procedure B.1 (Location Within Priority Land Use Districts) established by the Commercial Floor Area Allocation Procedures shall not be revised or amended for at least two years from the date of plan adoption.

The allocation procedures listed below identify how to acquire and use the additional 15,400 square feet of commercial floor area allocated to the community plan area. The allocation is derived from two sources: 10,000 square feet from the initial allocation; and 5,400 square feet of bonus commercial floor area. The allocations are intended to support additional development through 1996. At that time, the community plan will be reviewed and, based on amendments to the Regional Plan, may provide additional allocations of commercial floor area. In 1997 the Regional Plan was amended allocating an additional 10,000 square feet of commercial floor area to El Dorado County, bringing the total to 25,400 square feet. [§]

B. ALLOCATION PROCEDURES

The allocation procedures provide opportunities for all lands within the community plan area on which commercial uses are permitted to compete for an allocation of additional commercial floor area. To receive an allocation, project applicants must satisfy all of the following criteria.

1. Location Within Priority Land Use Districts. The community plan designates the Yank's Station and the West Meyers land use district as the priority areas (nodes) for additional commercial development. This includes additions to existing commercial uses as well as new development. The districts have relative advantages for increased development over the remainder of the plan area. Advantages include, but are not limited to, containing the majority of existing commercial uses, preferred locations for new retail commercial uses, containing additional area to expand and the ability to cluster development

[§] Amended 05/27/98.

thereby reducing vehicle trips. Refer to the Land Use Districts map in Chapter 2 for the exact boundaries of each district.

Establishing priorities clearly states the plan's intentions as to which geographic areas are most important for implementing the plan. Each priority contains a separate transfer ration required to use the allocation. Any of the priorities may be used at any time. In other words, it is not necessary to first build out all the commercial square footage provided under Priority #1 before moving on to Priority #2. The following priorities are established to guide the distribution of the additional floor area: §

Priority #1. Existing development or new development located within either Yank's Station or West Meyers land use districts. 15,000 square feet allocated. Match allocation using transfer ration shown below. §

Priority #2. Existing development located outside of the Yank's Station and West Meyers land use districts. 4,000 square feet allocated. Match allocation using transfer ratio shown below.

Priority #3. New Development located outside of the Yank's Station and West Meyers land use districts. 6,400 square feet allocated. Match allocation using transfer ration shown below. §

The Board of Supervisors, upon recommendation of the Planning Commission may modify the priorities and their related allocation without amendment to the Meyers Community Plan, provided the Board of Supervisors find that the allocations to the existing priorities are not adequately serving the needs and the policies of the Community Plan. §

New development is development which occurs on a vacant parcel. A vacant parcel as defined by TRPA is undeveloped or unimproved and has no established use. Existing development is development which is legally present or approved on the effective date of the community plan. Chapter 33 of TRPA's Code of Ordinances permits, under certain conditions, the expansion of legally existing commercial floor area by 5% or 500 square feet, whichever is less, without first receiving an allocation of new commercial floor area. The 5%/500 expansion may not be applied for or built in conjunction with any other addition or expansion. All commercial uses which can qualify for this expansion are encouraged to use it. Refer to TRPA Code Section 33.3 for the exact specifications for use.

In the event the additional commercial floor are previously allocated to El Dorado County outside of community plan areas is reallocated to the Meyers Community Plan (approximately 4,500 square feet), the allocation to each of the three priorities would be increased by the Board of Supervisors based on recommendation of the Planning Commission and their findings on how such allocations supports the Meyers Community Plan. §

2. Matching Commercial Floor Area. Allocation priorities identified above require

§ Amended 05/27/98.

each project applicant to transfer or otherwise bring to the project a certain amount of the project's total commercial square footage. It is possible to bring in more than the minimum required match, however, the maximum allocation of additional commercial floor area will not increase. The following matching ratios are established for each allocation priority:

- Priority #1: 75% allocation, 25% project match (3 to 1)
- Priority #2: 50% allocation, 50% project match (1 to 1)
- Priority #3: 25% allocation, 75% project match (1 to 3)

For the purpose of transferring matching floor area for a project, existing development (Priorities #1 and #2), may use legally created existing or banked commercial floor area which is on the parcel (or banked therefrom) as source of match. New development on a vacant parcel(s) must transfer matching floor area including banked floor area, from outside either the Yank's Station or West Meyers districts.

As an option to providing the match required herein, the applicant receiving the allocation may pay a fee of \$20.00 per square foot of required match. The funds received from this fee shall be collected by TRPA and shall be dispersed to El Dorado County in a manner similar to the County's existing air and water quality mitigation fund programs. Use of these funds shall be reserved for Capital Improvement Projects within the Meyers Community Plan.[§]

Using the allocation ratios and assuming all commercial floor area is built, a maximum of approximately 26,933 square feet of new commercial floor area may be developed as follows: 13,333 square feet in Priority #1 areas, 8,000 square feet in Priority #2 areas, and 5,600 square feet in Priority #3 areas.

The Ultimate amount developed may be somewhat less than this depending upon the source and location of the matching floor area.

3. Maximum Allocation Per Project. To make the allocation of additional commercial floor area available to as many projects as possible, a maximum of 2,000 square feet of additional commercial floor are will be allocated to any legally created parcel which is 6,000 square feet or larger as of July 1, 1987. Parcels which are smaller than 6,000 square feet as of July 1, 1987, may be eligible for an allocation which is proportional to the parcel size using the formula shown below.

$$\frac{\text{Parcel Size (s.f.)}}{6,000 \text{ s.f.}} = \frac{\text{Allocation of Floor Area (s.f.)}}{2,000 \text{ s.f.}}$$

4. Consistency with the Community Design Plan. Any project for which

[§] Amended 05/27.98.

additional commercial floor area is sought must fully implement the minimum community design elements identified in the Community Plan Design Review Guidelines (Appendix A, Section A). [§]The Planning Commission may, during the review of a proposed project, determine that additional design guidelines from either the Community Plan Design Guidelines or the TRPA Design Review Guidelines should be applied as conditions of project approval. El Dorado County will specify application, submittal and review procedures.

5. Timing and Distribution of Additional Commercial Floor Area Allocation. It is the intent of the plan that allocation of all of the additional commercial floor area be made within ninety days of community plan adoption. This will be done using a one time, filing deadline procedure. El Dorado County shall specify the application procedures, including the carry over of unused allocation and adjustments to matching ratios consistent with provisions of the Community Plan.

Contingency: In the case where applications are received requesting more commercial floor area than is available to allocate, [§]the Board of Supervisors or their designee with the assistance of TRPA staff, shall conduct a design competition. The competition shall be used to determine which project(s) best meet the needs of Meyers in terms of land use, physical design and other community improvements. El Dorado County shall establish the competition procedures.

C. IMPLEMENTATION

Project applications proposing to use additional commercial floor area allocated under the community plan will be reviewed by [§]the Planning Commission or Planning Department, as applicable, to determine consistency with the community plan in terms of its goals, objectives, policies and Community Design elements. They will forward their recommendations to El Dorado County and TRPA. Once received, TRPA staff will process the applications.

TRPA's Code of Ordinances establishes procedures for administering the additional floor area within a community plan. The allocation runs with the project meaning it may not be sold, conveyed or otherwise transferred to another project. Subsection 33.3.D is reprinted below as an aid to the reader.

- 33.3.D. Administration of Allocations For Additional Commercial Floor Area: For purposes of Subsection 33.3.C and for purposes of determining a rate of allocation in a community plan, the date of issuance by TRPA to a project of an allocation for additional commercial floor area establishes the year which the allocation is attributed.
 - (1) Allocations shall not be issued except in connection with project approvals. The date of issuance of the allocation is the date the project is approved by TRPA. The allocation shall be set forth in the approval of the project.

[§] Amended 12/20/95

- (2) An allocation for additional commercial floor area shall not be transferred to, or otherwise used for, a project other than that for which it was approved. If the allocation is not used prior to the expiration of the permit for the project, it shall expire with the permit, and the square footage of commercial floor area represented by the allocation shall automatically return to the pool from which it originated.
- (3) TRPA shall monitor the issuance, use and expiration of allocations to assure compliance with this chapter, and shall make periodic reports to the public, through the Governing Board, as to the status of the allocation of commercial floor area.

These procedures may be amended from time to time as necessary to implement the community plan.

Appendix D

ASSESSOR'S PARCEL DATA

Yank's Station Land Use District (Special Area #1)		
34-270-10	34-270-16	34-270-21
34-270-22	34-270-30	34-270-44
34-270-49	34-270-51	34-270-52 (53)
34-270-23 (54)	34-402-01	34-402-02
34-402-03	34-402-04	34-402-05
34-402-10	34-402-13	34-402-14
34-402-16	34-671-01	34-671-02
34-671-03	34-671-04	34-671-05
34-671-06	34-671-10	34-671-11
		27 Parcels
Lake Valley Land Use District (Special Area #2)		
34-040-03	34-040-04	34-300-08
34-331-23	34-331-24	34-341-06
34-341-07	34-342-02	34-342-04
34-342-05		
		10 Parcels
West Meyers Land Use District (Special Area #3)		
34-321-08	34-321-11	34-321-04
34-321-16	34-321-17	34-322-01
34-322-02	34-322-03	34-322-04
34-322-05	34-322-06	34-323-01
34-323-02	34-323-03	34-323-04
34-323-05	34-323-06	34-331-15
34-331-16	34-331-17	34-331-18
34-331-21	34-335-02	34-335-03
34-336-01	34-350-09	34-350-10
34-350-11	35-261-04	
		29 Parcels
Industrial Tract Land Use District (Special Area #4)		
35-030-19	35-181-05	35-181-06
35-181-07	35-181-08	35-181-09
35-182-01	35-182-02	35-182-03
35-182-04	35-182-05	35-182-06
35-183-01	35-183-02	35-183-03
35-183-04	35-183-05	35-183-06
35-183-07	35-183-08	35-183-09
35-183-10	35-183-11	35-183-12
35-191-01	35-191-02	35-191-03
35-191-04	35-192-01	35-192-02
35-192-03	35-192-04	35-192-05
35-192-09	35-193-01	35-193-02
35-193-03	35-193-04	35-193-07
35-193-08	35-193-09	35-194-01

35-261-05	35-261-06	35-262-01
35-262-02	35-262-03	35-262-04
35-262-05	35-262-06	35-262-07
35-262-08	35-262-09	35-262-10
35-262-11	35-262-12	35-262-13
35-262-14	35-262-15	35-263-01
35-263-02	35-263-03	35-263-04
35-264-01	35-264-02	35-264-03
		66 Parcels
Upper Truckee River Land Use District (Special Area #5)		
34-300-22 (21, 23)	34-300-25	34-300-26
34-300-27	34-300-28	35-211-01
35-211-02	35-211-03	35-211-04
35-211-05	35-211-06	35-211-10
35-211-11	35-211-12	35-211-13
35-211-15	35-211-16	35-211-17
35-211-18	35-211-19	35-211-23 (21, 22)
35-211-27	35-212-01	35-212-02
35-212-05	35-212-06	35-212-02
35-212-08	35-212-09	35-212-10
35-212-11	35-212-12	35-212-13
35-212-14	35-212-15	35-212-16
35-213-01	35-213-02	35-213-03
35-213-04	35-213-05	35-213-06
35-213-07	35-213-08	35-213-09
35-213-10	35-213-11	35-213-12
32-222-01	35-223-01	35-223-02
35-224-01		
		52 Parcels